

JONES & HERRIN

Architecture/Interior Design

02-16-89

Mr. E.J. Hauber
Trainmaster
1650 5th Street
Decatur, Al. 35601

Dear Mr. Hauber:

I was delighted to learn that the Norfolk-Southern is contemplating the restoration of the 1856 Memphis and Charleston R.R. Freight Depot in Huntsville (now N-S). In 1980 I inquired at the Smithsonian Museum whether they knew of any freight depot building which has been in continuous operation since 1856. They knew of none in the U.S., and just one in England. 139 years of continuous operation in a virtually unchanged building must surely be a record. I hope you would consider placing this unique structure on the Historic American Engineering Record, The National register of Historic Places, or both; it richly deserves such recognition. I'll be happy to furnish further information and donate help in the documentation and forms. In the process of researching the M. & C.R.R. for restoration of the adjacent 1860 passenger depot we have accumulated about 6 inches of historic photographs, maps, documents, and extensive 1980 photographs of the 1856 freight depot. a sampling of this information is attached for your use.

If I can help in any way I'd be glad to meet with you at the freight depot and donate some time toward the preservation and recognition of this important building.

Respectfully,



Harvie P. Jones, FAIA
HPJ/am

attachment:

Members of the American Institute of Architects
104 Jefferson Street Huntsville, Alabama 35801
Telephone 205/539-0764

CITY OF HUNTSVILLE HISTORIC PRESERVATION COMMISSION
P.O. BOX 308
HUNTSVILLE, AL. 35804
(205) 532-7475

August 20, 1990

Mr. J.R. Neikirk
Executive Vice President
Norfolk Southern Headquarters
3 Commercial Place
Norfolk, Va. 23510-2191

Dear Mr. Neikirk:

The City of Huntsville Historic Preservation Commission is aware of the national historical importance of the 1856 Memphis and Charleston R.R. Freight Depot in Huntsville, Alabama, now the Norfolk Southern Freight Depot. According to the Smithsonian Institution, this is the oldest known continually-operating freight depot in the United States. In the entire world, the Smithsonian knew of only one in England that was about 20 years older.

This 134 year old railroad structure is of solid, thick brick construction with a heartwood (decay resistant) heavy-timber roof frame. Many buildings in Huntsville and elsewhere of this type have been economically renovated to continue their useful life and to evidence the beginnings of the railroad industry in the U.S.

The City of Huntsville Historic Preservation Committee urges Norfolk Southern to continue an appropriate use of this solidly-built, historically important structure. A Huntsville preservation architect, Harvie P. Jones, FAIA, has offered to donate assistance in helping your facilities staff evaluate any needs of the structure. His telephone number is (205) 539-0764.

Please let us know if this Commission can be of assistance on this nationally important historic railroad structure.



Will A. Lewis, Chairman
408 Locust Avenue
Huntsville, Al. 35801

written by H. Jones for W.L.



Norfolk Southern Corporation
Three Commercial Place
Norfolk, Virginia 23510-2191
804 629-2610

Arnold B. McKinnon
Chairman, President and
Chief Executive Officer

February 28, 1991

→ Ms. Christine Purdy, President
Mr. Chuck Long, Board Member
Huntsville Art League
109-A Jefferson Street, Suite 111-B
Huntsville, Alabama 35801

Dear Ms. Purdy and Mr. Long:

This will acknowledge receipt of your letter of January 30 expressing an interest in the freight depot in Huntsville, Alabama. We have also had a request for this building from the Huntsville Depot Transportation Museum.

A portion of the building is currently leased to the local newspaper with the remainder occupied by our personnel. Although we expect to move some of our people, we plan to keep some in the building and so do not plan to give it up in the near future. When we do finally move out at some future date we will then decide what to do with the building. At that time, its proximity to our main track will create safety concerns for our continued operations if we permit its use for public purposes that we do not control.

We can well understand the historical significance of this depot and would certainly consider making it available to an organization such as yours or the transportation museum if it could be relocated to some other site. Since you are interested in preserving this structure, you may want to have someone look into this and determine how this can be accomplished. Any future correspondence should be addressed to Mr. R. E. L. deButts, our Executive Vice President Corporate Development and Real Estate, at this same address who has the responsibility of disposing of real property if it is no longer required for operations.

Sincerely,

A handwritten signature in dark ink, appearing to read "Arnold B. McKinnon".

cc: Mrs. Ginger Fail, Chairman
Historic Huntsville Foundation
P. O. Box 786
Huntsville, Alabama 35804

The Honorable Steve Hettinger, Mayor
City of Huntsville
P.O. Box 308
Huntsville, Alabama 35804

Huntsville Historic Preservation Commission
c/o Huntsville Building Inspection Department
P.O. Box 308
Huntsville, Al. 35804

August 6, 1991

State of Alabama Public Service Commission
P.O. Box 991
Montgomery, Al 36101-0991

Re: Docket No. 21985
(Proposed demolition of the 1856 Southern Railway Freight Depot
at Huntsville, Alabama)

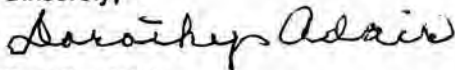
Dear Commission Members:

The Huntsville Historical Preservation Commission strongly opposes the request of the Southern Railway Company to destroy the 1856 Freight Depot at Huntsville, Alabama. According to the Railway Historian at the Smithsonian Institution in Washington, this is the oldest continually operating freight depot in the United States (135 years of continuous operation) and among the oldest in the world.

If the railway no longer needs the freight-handling facility here, at least this nationally important historic structure should be preserved. If the railway does not wish to restore and preserve it, the building should be given to an entity who will. Such an entity sits directly across the tracks - The Huntsville Depot Museum of the City of Huntsville. This museum consists of the 1860 Passenger Depot and Eastern Division Headquarters of the same railway, plus several other historic structures. It is fenced to preclude visitors from getting to the tracks. It has been owned by the City for nearly twenty years, with no problems relative to the railway operation. The Freight Depot could be similarly fenced, with a driveway/walkway connection to Church Street (about three hundred feet west) that would not interfere with the railway sidings to the north (see attached sketches).

Please do not allow this nationally-important historic landmark to be destroyed, especially when a painless solution, at no expense to the railway company or its customers, is at-hand.

Sincerely,



Dorothy Adair
Vice-Chairman, Huntsville Historical Preservation Commission
(in the absence of Chairman Will Lewis)
DA/kw

written by HS



C I R C A 1 8 6 0

THE HUNTSVILLE DEPOT

T R A N S P O R T A T I O N M U S E U M

320 Church Street Huntsville, AL. 35801 (205) 539-1860

CHAIRMAN
Guy J. Spencer, Jr.
VICE CHAIRMAN
Steve E. Tondera

September 9, 1991

Mr. John Baker, Resident Vice President
NORFOLK SOUTHERN CORPORATION
P.O. Box 1870
Montgomery, Alabama 36103

SUBJECT: Norfolk Southern freight building in Huntsville, Alabama.

Dear Mr. Baker:


I would like to begin by thanking you and Mr. Harres for meeting with all of us who are deeply concerned with Norfolk Southern's plans to close the Huntsville agency offices.

My concerns, being located directly across the tracks from and involved in the history of both our buildings, are centered around the preservation and future use of this remarkable structure. From the tone of the meeting and your statements at the meeting, most of our expressed fears have been put to rest. It would have been a tragedy for such an historic building to have been dismantled or destroyed.

With the excellent probability that Norfolk Southern and the City of Huntsville will come to an agreement on the disposition of the freight building, it is my request that Norfolk Southern give permission and assistance to have the structure placed on the National Register of Historic Places. Necessary historical data required by the Department of the Interior has been gathered and researched, making the procedure relatively simple and timely. As stated above, I will need Norfolk Southern's permission in order to begin.

Your consideration and assistance is appreciated.

Sincerely,


Bryant A. Bienvenu
Director

cc: Mr. Jim Putnam, President
Huntsville City Council

Congressman Bud Cramer
✓ Mr. Harvey Jones
Mr. Ronnie Wade

Handwritten notes:
meet
A Jones
Biemvenu
4/14/77 in pres
at all

Handwritten notes:
11/2
suggestion

PEPSI
THE CHOICE OF A NEW GENERATION





**NORFOLK
SOUTHERN**

Norfolk Southern Corporation
RSA Plaza
770 Washington Avenue, Suite 184
P. O. Box 1870
Montgomery, Alabama 36102-1870

John Baker
Resident Vice President - Public Affairs

Phone (205) 262-7602
Facsimile (205) 262-6853

September 11, 1991

Mr. Bryant A. Bienvenu
The Huntsville Depot Transportation Museum
320 Church Street
Huntsville, Alabama 35801

Dear Mr. Bienvenu:

Thank you for your letter of September 9. I am pleased that we were able to have the meeting you referred to in Huntsville so that we had an opportunity to assure you others interested that Norfolk Southern is willing to make the Huntsville Freight Depot available to interested parties for historic preservation.

However, I tried to stress the fact that Norfolk Southern strongly desires to close the agency which is presently located in the building. Once that is done, Norfolk Southern does not desire to continue to own and maintain the building. Certainly, Norfolk Southern's willingness to see the building preserved depends upon some group or organization coming forward very soon to work out the details of a transfer. To that end, I encourage your organization or some local organization to take immediate steps to make a proposal for acceptance of the building.

As I explained, Norfolk Southern would like to retain title to the underlying land because of the location of the land in between our main line and our yard tracks. In the event that the building is ever completely destroyed by fire or storm or is abandoned and not maintained as a historical site, then Norfolk Southern would like to reclaim the land. For that reason, I suggested at the meeting that Norfolk Southern management would be far more receptive to a proposal whereby Norfolk Southern donates the building and leases the underlying land. Our standard lease has a termination clause which could be exercised by either party and would also require certain fencing and insurance.

Please keep in mind that I am not authorized to make such a proposal as an offer. However, I believe our management would entertain a proposal on that basis.

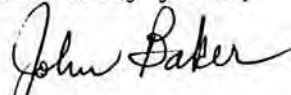
Sup. 2

Page 2

Mr. Bryant A. Bienvenu

There is a "window of opportunity" at the present time wherein our management has an incentive to conclude such an arrangement in a relatively short period of time. If that is done, then the new owner could take whatever steps they desire to have the building placed on the National Register of Historic Places.

Sincerely yours,



John Baker

cys: Mr. Jim Putnam
Congressman Bud Cramer
Mr. Harvey Jones
Mr. Ronnie Wade

JONES & HERRIN

Architecture/Interior Design

September 24, 1991

Bud Bienvenu, Executive Director
The Huntsville Depot Museum
320 Church Street N.W.
Huntsville, Al. 35801

Re: The Huntsville Depot Transportation Museum

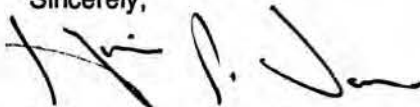
Dear Bud:

Due to post-1856 buildup of the finish-grades adjoining the M & C (Norfolk-Southern) tracks that run beside the 1856 freight depot of the 1860 passenger depot, water is trapped in the roadbed and can only dissipate by soaking into the soil adjoining the two historic buildings. During restoration of the 1860 building we discovered many layers of different-color painted wainscots of about 4 feet high on the building exterior that attested to frequent repainting of the bottom of the wall where the ground-water rises up in the porous 1860 bricks ("rising damp") and blisters the paint off the bottom few feet of the walls. This process of rising damp has also deteriorated the mortar in the bottom few feet of the 1856 freight depot south and west walls, aggravated by digging a sump near the building years ago to obtain a higher loading dock by lowering the soil, thus creating another pond next to the west end of the freight building.

In the fall of 1990 we installed a moisture barrier in the bottom of the porous brick walls of the 1819 Weeden House, which has long suffered from rising damp. This process is a chemical injection by a company named Pro-So-Co., who specialize in historic masonry restoration. While a winter's experience is needed to further test the results, so far they look promising. It would be good to consider this system for the 1856 and 1860 buildings.

The "truck pit" at the loading dock should be filled as soon as possible. This should stop this problem at the west wall of the 1856 building, where it is most severe. Proper tuck-pointing will be needed for the lower bricks here. Since there are wrong and right ways to do this, I'll be happy to help guide the mason when this is to be done. It is a technique that masons do not know.

Sincerely,



Harvie P. Jones, FAIA
HPJ/am

cc: file



Norfolk Southern Corporation
RSA Plaza
770 Washington Avenue, Suite 184
P. O. Box 1870
Montgomery, Alabama 36102-1870

John Baker
Resident Vice President - Public Affairs
Phone (205) 262-7602
Facsimile (205) 262-6853

November 12, 1991

Honorable Steve Hettinger
Mayor, City of Huntsville
Post Office Box 308
Huntsville, Alabama 35804

Subject: Norfolk Southern Huntsville Agency and Freight Depot

Dear Mayor Hettinger: *Steve*

I wanted to bring you up to date on Norfolk Southern's interest in closing the agency and freight depot in Huntsville.

First of all, let me assure you that this action by Norfolk Southern does not mean that we are reducing our presence in Huntsville or the level of business operations there. To the contrary, with new technology such as computers, fax machines and paperless transactions, we will be able to serve our customers more efficiently. We see Huntsville and the entire Tennessee Valley as a great opportunity for Norfolk Southern. Our effort to close the agency is to modernize Norfolk Southern and to make us more efficient and competitive.

Norfolk Southern is aware of the historical significance of the depot. The original application which we filed with the Public Service Commission had boiler plate language requesting that upon closing the agency we be allowed to dismantle and remove the freight depot. That language simply is inapplicable to the situation at Huntsville.

Our management is interested in historical preservation. Norfolk Southern would be willing to donate the old freight depot to the city or to an appropriate historical preservation organization. We would lease the land to the same donee with standard provisions for insurance to protect Norfolk Southern and for the erection of fences on the property line.

Norfolk Southern presently has a portion of that building leased to a tenant whose rights will also have to be considered. Norfolk Southern would retain title to the underlying real estate. The freight depot is located on a pie shaped tract with our mainline tracks on one side and our freight yard on the other side. If the building is ever totally destroyed by fire or storm, we would reclaim possession of the land.

CC-AH-AS

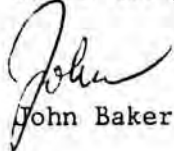
Page 2
Honorable Steve Hettinger
November 12, 1991

Our lease of the land would provide for a short termination clause by either party in the event of changed circumstances.

In order to proceed with our request to make this business change, we will withdraw the application filed with the Alabama Public Service Commission that referred to dismantling and removing the freight depot. We will file a new application that will set out our proposal to preserve this historic structure. In our new application we will propose that the Public Service Commission grant a period of time after the final order, of at least two years, during which time the city or interested organizations could come forward to accept donation of the building. If there is no interest by the city or historical groups in preserving the building, we would want permission to eventually dismantle and remove the building.

Filing of the new application, posting, and giving notice of that filing has not yet taken place but will probably take place within the next 30 days. I wanted you to have advance notice of the procedure we propose to follow.

Yours truly,



John Baker

cys: Honorable Jim Putnam
Mr. Ronnie Wade
Mr. Warren P. Jones
Mr. Roger M. Nicholson
Mr. Harvie P. Jones
Mr. Gerald Patterson
Mr. Will Lewis
Ms. Dorothy Adair
Mr. Bud Bienvenu

JONES & HERRIN

Architecture/Interior Design

August 31, 1993

Huntsville City Planning Commission
P. O. Box 308
Huntsville, Alabama 35804

Attn: Mr. Dallas Fanning, Director
Ms. Linda Allen, Planner

Re: The 1856 Memphis & Charleston Railroad Freight Depot
Norfolk-Southern Railway


Dear Mr. Fanning and Ms. Allen:

1. The 1856 Memphis and Charleston Railroad Freight Depot has an enclosed floor area of about 5,400 square feet.
2. The platform area is about 3,900 square feet.
3. The roof area is about 10,000 square feet, normal to the roof planes.
4. It is proposed at this time to put the building in good repair in its present configuration (i.e. no HVAC system, insulation, or other such improvements). The present roof material is assumed to be retained (cement-asbestos shingles) since this is an excellent material, although a determination as to its environmental safety must be made. Most literature on this subject now recommends covering such material rather than disturbing (removing) it, such as with masonry paint.
5. A detailed examination of the building has not been made available, so any cost projections must be based on this firm's considerable past experience in restoration, including that of an 1886 freight depot in Albertville and the 1860 Memphis & Charleston Eastern Division Offices and Passenger Terminal in Huntsville, Alabama.
6. Restoration without improvements (paragraph 4) could range between \$150,000 and \$250,000.
7. To completely restore the building and platforms, replace the roof shingles, provide a fifteen car parking lot, modern HVAC system, insulation, plumbing, wiring etc. would probably range from \$270,000 to \$405,000, depending on needed modifications and a detailed assessment of repairs. This is about \$75 per square foot of heated area at the high end of the range. This firm has recently restored an elaborate but very dilapidated 1905 Carnegie Library that was set for demolition for only \$56 per square foot, so this cost range of \$50 to \$75 per square foot seems conservative for this simple brick and heavy-timber depot.
8. A detailed assessment of the building should be made to more accurately project a possible restoration cost.

9. This firm has found (in our six-hundred restoration projects in four states) that in restoration of buildings thought to be "too far gone", complete restoration consistently is far less costly than modern new construction of the same floor area done in a standard modern design (not a replication of the historic building).

10. The 1856 Memphis & Charleston Freight Depot is, per the Smithsonian Institution's railroad expert in 1979, the oldest continually operating freight depot in the United States, a high distinction. It behooves Huntsville to not let this nationally-important historic structure slip away.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

cc: HJ

John Baker - Not Saw 222
tel com 2 Sept. 93 (H)

Celia _____ (Week)

AUG. 93
Re 1856
Deposit

~~to Herby~~

Paul Gibson

Supt of Tr. Div., Norfolk Son.
7208 Old Pottledge Pk
Knox. Tn 37914

(615) - ~~521-1449~~
521 1435

FAX 615-521-1500

P. E. Gibson Jr.

P.O. Box 14820 Knox, Tn 37914

put in
letter:

James. certifying
Options -

by N-S, revins. by HHS (work by Tig Tug)
by HAF - permission by N-S.

J. Baker
says

Parking = o.k. — land = flexible boundary, w/ parking
Fences rough. Survey out land desired.

Av. Trees needs space for, stor. — least needed. How much
space?

262-7602

John Baker
Brown
(Norfolk Son. 222)



JONES & HERRIN

Architecture/Interior Design

September 9, 1993

Mr. P. E. Gibson, Jr.
Norfolk Southern
7208 Old Rutledge Pike
Knoxville, Tennessee 37914
Fax: 615-521-1500

Re: 1856 Freight Depot, Huntsville, Alabama

Dear Mr. Gibson:

Per our telephone conversation of 9 September, the Historic Huntsville Foundation is working on a way to save the 1856 Freight Depot here, the oldest in continuous operation in the United States per the Smithsonian Institution. The immediate need is to stop deterioration at the building due to some loose roof shingles. The Historic Huntsville Foundation has donated up to one-thousand dollars (\$1,000) to perform temporary repairs by Tip Top Roofing, our most reputable and oldest roofing company. A copy of their insurance certificate is attached. Tip Top will furnish a certificate to you specifically written for this project before the work begins. Tip Top estimates about one-half (1/2) to one (1) days work.

Please let us know if we can proceed on this.

Thanks,



Harvie P. Jones, FAIA
Member, Historic Huntsville Foundation
HPJ/tm

cc: Ms. Suzanne O'Connor
Historic Huntsville Foundation President

HJ

attachment

JONES & HERRIN

Architecture/Interior Design

November 30, 1993

Norfolk Southern Company
Attn: Mr. John Baker, Vice President of Public Affairs
RSA Plaza
770 Washington Avenue
Suite 184
P. O. Box 1870
Montgomery, Alabama 36102-1870

Re: 1856 Huntsville Freight Depot

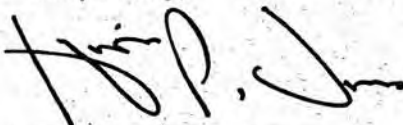
Dear Mr. Baker:

Attached are two original executed copies of the Norfolk Southern indemnification agreement, signed by William G. Steed, President of Tip-Top Roofing of Huntsville, dated November 11, 1993.

Please let me know if this allows Tip Top Roofing to execute the minor roof patching at the historic 1856 Norfolk Southern Freight Depot at the expense (approximately \$1,000) of the Historic Huntsville Foundation. The objective of the Historic Huntsville Foundation is to stop deterioration while a permanent use of the building is worked out. The Foundation is very anxious to preserve this nationally important building, which is the oldest continuously-operating freight depot in the United States, per the Smithsonian Institution.

We greatly appreciate your interest and assistance.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

cc: H.J.
Huntsville Historic Foundation

PERMIT AND INDEMNIFICATION AGREEMENT

I. Indemnity Agreement

I, WILLIAM G. STEED acting on behalf of Tip Top Roofing & Sheet Metal and its employees have requested and do request permission of Norfolk Southern Railway Company (referred to hereinafter as "Company") to enter and be present upon equipment, rolling stock, premises, property and/or facilities owned, and/or facilities owned, leased or controlled by Company (referred to hereinafter as "Company Property"), at and in the vicinity of HUNTSVILLE, Alabama, depot for the purpose of repairing its roof, on , 1993, or from Dec. 16, 1993, to March 31, 1994.

In consideration of the consent of Company to my entry and presence upon Company Property, I agree and undertake to:

- (1) Give advance notice to Company's officer or agent signing this Agreement or his or her authorized representative of each entry upon Company Property;
- (2) Accept direction from Company's supervision while on Company Property;
- (3) Assume all risk of injury to me (including death) and damage to my property or property in my custody or control arising directly or indirectly from my presence on Company Property; and
- (4) Indemnify and forever save harmless Company and the Other Indemnified Parties described below from and against any and all loss, costs, claims, liability, damage and expense resulting from injury to or death of persons, including but not limited to myself, and damage to or loss of property, including but not limited to property in my possession or ownership, caused by or arising or in any manner growing out of or resulting in whole or in part from, directly or indirectly, my presence on Company Property, regardless of any negligence of Company or any Other Indemnified Party contributing thereto.

I know that railroad operations involve particular risks and dangers. If my being on Company Property exposes me to any railroad operations, I specifically assume all risk of injury (including death) and damage from railroad operations as part of the above assumption of risk.

The Other Indemnified Parties mentioned above are all of Company's corporate affiliates, subsidiaries and parent (if any) and all officers, agents and employees of Company and such other subsidiaries, affiliates and parent.

I further agree that neither I nor my heirs, executors, administrators, assigns, or successors shall cause any claim or lawsuit to be brought for any injury (including death) I may sustain, directly or indirectly, as a result of my presence on Company Property.

I also agree that this agreement shall be binding upon my heirs, executors, administrators, successors and assigns and that if any term or provision of this agreement shall to any extent be held invalid or unenforceable, the remaining terms of the agreement shall not be affected thereby, but shall be valid and enforceable to the fullest extent permitted by law.

In witness of my understanding of the foregoing and to evidence my representation that I HAVE READ THIS ENTIRE PERMIT AND INDEMNIFICATION AGREEMENT AND UNDERSTAND AND ACCEPT AND AGREE TO ITS TERMS, I have executed this writing the 11 day of NOVEMBER, 1993.

Witness:

Joyce M. Walker

TIP TOP ROOFING & SHEET METAL,
By William G. Steed, PRESIDENT
Title:

II. Permit

In consideration of the above indemnity agreement personally executed by William G. Steed, permission is hereby granted to TIP TOP ROOFING & SHEET METAL, to be upon the Company Property at or in the vicinity of HUNTSVILLE, Alabama depot, on Dec. 15, 1993.

NORFOLK SOUTHERN RAILWAY COMPANY
By P. E. Gibson, Jr.
Title Division Superintendent
City and State Knoxville, Tennessee

Date: Dec. 15, 1993



**NORFOLK
SOUTHERN**

Norfolk Southern Corporation
7208 Old Rutledge Pike
Post Office Box 14820
Knoxville, Tennessee 37914-1820
615 521-1400

Paul E. Gibson, Jr.
Division Superintendent

December 15, 1993

Jones & Herrin
Architecture/Interior Design
Attn: Harvie P. Jones, FAIA
104 Jefferson St.
Huntsville, AL. 35801

Dear Mr. Jones:

Attached find one original signed and executed copy of the Norfolk Southern Permit and Indemnification Agreement. This gives you permission to repair the roof of our freight depot at Huntsville, Al. As mentioned in your letter dated September 9, 1993, this worked will be done with up to \$1000.00 donated by the Historic Huntsville Foundation. With this letter the Norfolk Southern assumes no responsibility for the cost of the repairs, repairs will be done at no expense to the Norfolk Southern.

I understand Tip Top will furnish certificate on this project before the work begins. If repairs cannot be made in the time frame shown on the agreement, please notify this office in writing of time needed.

Yours truly

P. E. Gibson, Jr.
Superintendent
gc

JONES & HERRIN

Architecture/Interior Design

Dec 21 '93 Harvie P. Jones, F.A.I.A.

to Tig Top Roofing
attn: Jerry Stahl

re 1456 W. Freight Depot
(Horseshoe Soc. RR)

Jerry - Attached dated
15 Dec. '93 is your original
signed copy of the "Permit
and Indemnification Agree."
to do temporary roof patching
for \$1,000 max., to be paid by
the W. Fr. Depot.

Please send insur. certif. to
N.S. Co. before proceeding.
Work to be completed by
31 Mar '94.

Thanks! Harvie

104 Jefferson Street
Huntsville, Alabama 35801
Telephone 205/539-0764
Fax 205/534-2289

C-14J
HHF

JONES & HERRIN

Architecture/Interior Design

May 2, 1994

Historic American Engineering Record
National Parks Service
Washington, D. C. , 20013-7127

Attn: Mr. Eric Deloney *Deloney*


Re: 1856 Huntsville Freight Depot
1873 Stone-arch Rail Road Bridge

Dear Eric:

Attached are location maps etc. for the two subject structures. I hope your HAER team will be able to do some survey work on these two structures this summer. Surely both are rare survivors in terms of longevity of use for their types, since both are still functioning for their original purpose. The bridge is particularly beautiful, as well. The freight depot, per the Smithsonian's rail-road expert, is the oldest in the United States in continuous use.

Please let me know if I can help further. If your group comes here, I'll be happy to show them around. Our three historic districts contain about five-hundred buildings, dating to 1814.

Thanks!


Harvie P. Jones, FAIA
HPJ/tm

cc: HJ

attachments

JONES & HERRIN

Architecture/Interior Design

16 June 94 Harvie P. Jones, F.A.I.A.

^{DELOVY}
Eric DeLovy, Dir.

Historic Am. Engineering Record
(HAER)

Nat. Parks Service

Wash. D.C. 20013-7127

Dear Eric —

Many THANKS for your
efforts of persistence on this!

As I recall, we began
work on this in 1985 or
before.

We appreciate your good
work. Let me know if I
can be of any coord. help.



104 Jefferson Street
Huntsville, Alabama 35801
Telephone 205/539-0764
Fax 205/534-2289

nc



United States Department of the Interior



NATIONAL PARK SERVICE

P.O. Box 37127

Washington, D.C. 20013-7127

IN REPLY REFER TO:

H3815 (0429)

October 7, 1994

Harvie P. Jones, FAIA
Jones & Herrin Architects Interior Designers
104 Jefferson Street
Huntsville, Alabama 35801

Dear Harvie,

I am writing with some good news from Washington. Both the House and the Senate passed and, last Friday, September 30th, President Clinton signed the 1995 Appropriations Bill for the Department of the Interior. One of the provisions in the bill is \$100,000 for "HABS/HAER activities in Muscle Shoals and Huntsville, Alabama."

It seems like the time you were able to share with Richard O'Connor and me on August 23rd was well spent. Richard and Mark Schara, the head of our CAD/Photogrammetry laboratory, will be travelling to the Huntsville area the week of October 17th to begin reconnaissance of potential sites to document. One Huntsville site I know they want to record with CAD/Photogrammetry is the single span stone arch railroad bridge you told Richard about that is west of Huntsville.

I also want to thank you and your wife for the personalized tour of historic buildings in Huntsville and the wonderful dinner we shared at Richards. I had never been to northern Alabama before that trip and you and the other Alabamians I met over the next two days showed me an astonishing array of historic architecture and engineering. From the stately homes and grace of Huntsville, to the dramatic forms of Wilson Dam and the Rosenbaum House in Muscle Shoals, to the raw power of Birmingham's iron industry, I learned a lot about the heritage of the state.

Thanks again for sharing your knowledge of the Huntsville area.

Sincerely,

John A. Burns, AIA
Deputy Chief
Historic American Buildings Survey/
Historic American Engineering Record

JONES & HERRIN

Architecture/Interior Design

January 6, 1995

Mr. Cooper Green, Marketing Director
c/o Huntsville Times
2317 South Memorial Parkway
Huntsville, Alabama 35801

Re: 1856 Memphis and Charleston Railroad Freight Depot
(now Norfolk-Southern Railroad)
Huntsville, Alabama

Dear Mr. Green:

As you are aware, the railroad history specialist at the Smithsonian Institution in Washington D.C. some years ago said that to his knowledge the 1856 Memphis and Charleston Railroad freight depot in Huntsville is the oldest one in continuous operation in the United States. He knew of only one anywhere (England, circa 1835) slightly older. Therefore this structure is of considerable national and even international significance and should be preserved as an important component of America's railroading history.

Probably the best approach to its preservation is to (1) keep it handling freight or (2) use it for some compatible adaptive use that would not entail major modifications to its exterior or interior. This office did such an adaptive use for an 1886 depot in Albertville, Alabama and hundreds of depots have been successfully put to similar uses. Athens has a similar example. Safety can be managed easily by fencing, as we did in 1977 at the adjacent 1860 Memphis and Charleston passenger depot museum.

In 1994 the Historic Huntsville Foundation donated \$2,000 to patch the roof of the freight depot, which had numerous missing cement-fiber shingles, primarily at the eaves. This has helped considerably to slow down possible water damage. Luckily, the wood used in 1856 has excellent rot-resistance and the wood damage appears to be minimal. The \$2,000 did not cover the patching of an area of about twenty missing shingles on the south slope. This needs to be patched in the near-term future.

In the mid-20th century a concrete-block and concrete slab platform was built that extends fifty or so feet west of the southwest corner of the building parallel to the siding tracks. This has created a water-trap by the west wall of the building where water stands constantly. This moisture soaks into the soil and thence migrates by capillary action up into the porous 1856 brick walls, a process called "rising damp". This rising damp has deteriorated (by constant wetting and drying) the 1856 lime-and-sand mortar at the lower part of the bricks at the west end of the building and to a slight degree along the south track side. This problem is easily solved by eliminating the cause - the water trap; and then properly re-laying the small areas of loose bricks and repointing the soft mortar with lime-and-sand mortar matching the original (modern Portland-cement mortar is hard and brittle and should not be used in a historic building).

The building should now at a minimum be "stabilized", a term in historic preservation meaning to stop the damage and make the building safe to the greatest degree practicable with limited funds.

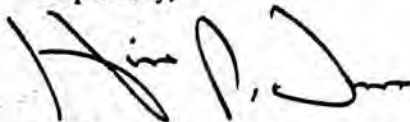
1. Drain the pond. The easiest way is to fill against the platform to shed the water to the north-west. This would cause no functional problem since the platform extension no longer serves as a loading dock. An option is to demolish the now non-functional platform extension. It appears, short of obtaining a topographic survey, that the filling option would function best.
2. About 250 square feet of loose brick wall area needs to be re-laid as noted above (with guidance from a historic preservation architect; we will be happy to donate this).
3. The remaining gap in the roof shingles needs to be patched with galvanized sheet-metal to exclude water entry. Since this area is out in the middle of the south slope, access must be done with care since it is possible some decking in the area is decayed. If some decay is found, the area should be strengthened underneath with temporary wood scabs.
4. A qualified electrical inspector should also look over the electrical system to try to get a sense of the probable safety of the system. It should be recognized that no inspector can see every possible problem spot.

The above procedures will do nothing for the appearance of the building. This work would come in a "restoration" phase, as well as proper repairs versus stabilization patching.

While there is not enough detailed information presently available for a detailed labor and material costing, it is my opinion that the above stabilization work might be done for a range of about \$25,000-\$35,000. It would be best to refine this by first deciding on a procedure schedule, engaging a qualified restoration contractor (at least three are in Huntsville) and having that contractor work up a detailed estimate based on a more detailed description of work. It is probably cheapest to have the contractor do the work based on a fixed dollar fee over actual cost, since many quantities will be subject to much guesswork for a fixed-cost approach, and contractors try to guess high if forced to guess.

I hope this information will be of assistance in planning for the preservation of the important building. Please call if I can furnish further information.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

copy: HJ

The Huntsville Times

P.O. BOX 1487-WEST STATION
HUNTSVILLE, ALABAMA 35807
(205) 532-4000

WM. COOPER GREEN, III
GENERAL MANAGER

January 20, 1995

COPY

Mr. Lee Burkett
Burkett Construction
P. O. Box 1441
Huntsville, Alabama 35807

Dear Mr. Burkett:

This is a request for the bid which we discussed last week. As you know the work that needs to be done is at the Norfolk Southern Freight Depot, 330 Church Street N.W..

Please submit your estimate by February 10, 1995, so that we may resolve any remaining questions prior to our February 23 hearing with Norfolk Southern and the Public Service Commission.

Attached are inspection copies and accompanying photographs from Harvie Jones.

If you have any questions please contact Mr. Jones or myself.

Very truly yours,


Wm. Cooper Green, III

WCG/kd

enclosures

cc: Bob Ludwig, Publisher
Harvie Jones, Jones & Herrin Architects

The Huntsville Times

P.O. BOX 1487-WEST STATION
HUNTSVILLE, ALABAMA 35807
(205) 532-4000

WM. COOPER GREEN, III
GENERAL MANAGER

January 20, 1995

COPY

Mr. Frank Vandiver
Vandiver Construction
1854 Keats Drive
Huntsville, Alabama 35810

Dear Mr. Vandiver:

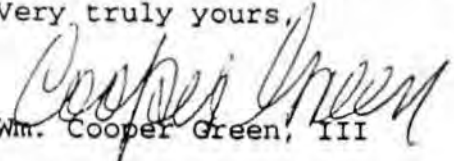
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If you have any questions please contact Mr. Jones or myself.

Very truly yours,


Wm. Cooper Green, III

WCG/kd

enclosures

cc: Bob Ludwig, Publisher
Harvie Jones, Jones & Herrin Architects

JONES & HERRIN

Architecture/Interior Design

January 24, 1995


Copy
Mr. William ~~George~~ Green, III
General Manager
The Huntsville Times
P.O. Box 1487 - West Station
Huntsville, Alabama 35807

Re: 1856 Memphis and Chattanooga Railroad Freight Depot

Dear Mr. Green:

I'm delighted to see that you are soliciting bids for stabilization repairs at the 1856 Depot. I would be happy to review the proposed work and techniques with the successful contractor and to try to answer questions. This building is too important to let it drift out of existence.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

copy: hj

VANDIVER

R.F. VANDIVER CONSTRUCTION CO., INC.
1854 KEATS DRIVE
HUNTSVILLE, AL 35810
(205) 859-4509

January 30, 1995

The Huntsville Times
2317 Memorial Parkway, SW
Huntsville, Alabama 35801

Attention: Mr. Cooper Green III

Referencing: Estimate Proposal For the
Norfolk Southern Freight Depot

Dear Mr. Green:

I am in receipt of your bid package proposal for the subject project. We appreciate the opportunity to offer a proposal on this matter. We have past experience with Harvey Jones on historic restoration projects.

In fact, I have managed previous work associated with the main depot which included the roof replacement.

We would offer a fixed fee in the amount of \$5,000.00 for work as identified in your bid package. Further, unclassified work could be performed at an agreed upon cost plus a fee basis.

Once again, we appreciate the opportunity to offer this proposal. If you have any questions or need additional information please do not hesitate to contact me. We would gladly entertain meeting with you at your convenience to further discuss.

Respectfully,



Donald Hellen
Project Manager

DH/lj

COPY

The Huntsville Times

P.O. BOX 1487-WEST STATION
HUNTSVILLE, ALABAMA 35807
(205) 532-4000

WM. COOPER GREEN, III
GENERAL MANAGER

May 19, 1995

Mr. Harvie Jones
Jones and Herrin Architects
194 Jefferson Street
Huntsville, Alabama 35801

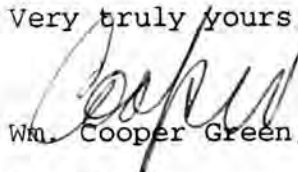
Dear Mr. Jones:

Thanks for your continued direction on what needs to be done at the railroad depot in order to stabilize its deterioration.

A request for bid sent to Vandiver Construction and Burkett Construction yielded only one response. Vandiver submitted a brief non-specific bid of \$5,000 (copy enclosed). Although Lee Burkett stated on two occasions that he would submit a bid, I have yet to hear from him.

What do you recommend? Are there other contractors that I should be contacting? What is your opinion of the Vandiver bid? Please let me hear from you at your earliest convenience.

Very truly yours,


Wm. Cooper Green, III

WCG/kd

enclosure

JONES & HERRIN

Architecture/Interior Design

May 30, 1995

"The Huntsville Times"
P.O. Box 1487 West Street
Huntsville, Alabama 35807

Attn: Mr. Cooper Green, General Manager

Re: 1856 Memphis & Charleston Railroad Freight Depot

Dear Mr. Green:

Attached for your review and comment is a design for the correction of the ponding by the 1856 Memphis & Charleston Railroad Freight Depot. Only the sump pump information is lacking. I hope to have this information by June 10 from our engineer.

A guess is that this may cost in the range of \$12,000-\$16,000. With the pump information added on the plans, you should be able to get a fixed bid on this item.

Note that, per our telephone discussion, the Huntsville Times is to extend electrical power to the sump pump, in a manner that lift-trucks won't damage the conduit.

There will still be some ponding west of the new pad, but this is unavoidable. The object is to keep the ponding away from the 1856 building to reduce "rising damp" damage to the building.

In heavy rains, it will take time for the pump to empty the pad recess, but the concrete will prevent the water from soaking into the ground, achieving the object stated above.

This work is a donation of the Historic Huntsville Foundation and Jones & Herrin. We greatly appreciate the Huntsville Times's efforts to preserve this nationally-important building.

Let me know if any changes are needed. I will get the pump information to you when I receive it.

Thanks!



Harvie P. Jones, FAIA
HPJ/tm

copy: Mr. Louie Noe
HJ

attachment

JONES & HERRIN

Architecture/Interior Design

May 30, 1995

Mr. Louie Noe
Miller & Weaver, Inc.
2720 Third Avenue, South
Birmingham, Alabama 35233

Re: 1856 Memphis & Charleston Railroad Freight Depot

Dear Louie:

Attached is a print of the depressed loading dock pad for which a sump-pump is needed. The drainage area is approximately 2,000 square feet. No roof water spills in here. Surface water is diverted.

The pump should be tolerant of limestone dust etc. since the adjoining truck area is surfaced with crushed limestone on soil, and some will get in here in wet weather, on tires.

Please let me know what the power connection should be as well. The owner is to run the power to the sump.

Bill us on an hourly basis. My understanding is that you expect it not to exceed +/- 2 hours, which has been approved by the "Huntsville Times", who is doing this on a civic basis, as are we.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

copy: Mr. William Cooper, III, General Manager, Huntsville Times

 ^{Green}

attachment



Miller & Weaver, Inc.
CONSULTING ENGINEERS

2720 3rd Avenue South
Birmingham, Alabama 35233

Phone (205) 252-0246
FAX (205) 251-8506

June 8, 1995

Jones & Herrin
104 Jefferson Street
Huntsville, AL 35801

Attn: Harvie Jones

Re: 1856 Memphis & Charleston R.R. Freight Depot
M&W 95152

Dear Harvie:

Attached is a copy of a portion of your plan with notes concerning the sump pump.

The pump should be equal to a Weil Pump Co. Model 2435, 1/2 HP 208 or 230 volt single phase, with differential float switch with a minimum capacity of 100 GPM at 7 foot head.

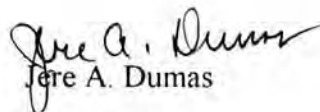
The minimum sump dimensions should be 24" diameter x 3'-0" deep.

The discharge pipe should be 3" galvanized steel securely anchored to sump wall and top of curb.

If you have any questions, do not hesitate to call.

Very truly yours,

MILLER & WEAVER, INC.


Jere A. Dumas

Attachment

/da

BURKETT CONSTRUCTION COMPANY, INC.

P.O. Box 1441

Huntsville, AL 35807

Tel. (205) 536-9351

June 9, 1995

Huntsville Times
P.O. Box 1487
Huntsville, AL 35807

ATTN: Mr. William Cooper Green, III

RE: Bid for Work on the Freight Depot

Dear Sir:

We have worked up a price for accomplishing the following work on the Freight Depot.

1. Clean out the gutters.
2. Rod out the downspouts.
3. Repair the masonry (Ezell Connerly) at the west end, and parts of the north and south sides on the west portion of the building.
4. Install a sheetmetal patch over the hole in the roof.
5. Install approximately 200 liner feet of fascia flashing.
6. Install a sump pump in a basin to drain the water from the loading dock area. Slope the earth (gravel) to the basin. Pump water to the south side of the dock.

Our price is \$18,000.00 (Eighteen thousand dollars and no cents). We agree to hold our bid open for 30 days and have not included a bond.

If you have any questions please feel free to call at your convenience.

Sincerely,
BURKETT CONSTRUCTION COMPANY, INC.



LEE W. BURKETT
President

JONES & HERRIN

Architecture/Interior Design

June 12, 1995

Mr. Cooper Green, General Manager
"The Huntsville Times"
P.O. Box 1487 West Station
Huntsville, Alabama 35807

Re: 1856 Memphis & Charleston Railroad Freight Depot
Project No. 95103

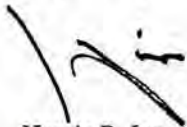
Dear Mr. Green:

Attached for your use are five prints of the depot loading pad dated May 27th, and revised June 12, 1995 to include the pump and sump information supplied by Miller & Weaver, Engineers of Birmingham.

Please call if you have any questions on this or the other repairs. I definitely need to meet with the mason who is to do the brick repairs, for I've found that drawings and specs are not enough to get the points across.

We are happy to make this donation to assist in preserving this important historic building.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

copy: HJ

attachment

The Huntsville Times

P.O. BOX 1487-WEST STATION
HUNTSVILLE, ALABAMA 35807
(205) 532-4000
June 12, 1995

WM. COOPER GREEN, III
GENERAL MANAGER

To: Mr. Harvie Jones
Jones & Herrin Architects
104 Jefferson Street
Huntsville, AL 35801

Subject: Burkett Construction Railroad Depot Bid

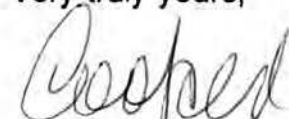
Attached is a copy of the Burkett Construction bid for repairing/stabilizing the deterioration of the old railroad depot. Please review it and advise me as to how we should proceed.

Two questions which come to mind:

- 1) I notice it calls for a gravel rather than concrete loading area. How much more will the concrete ramp cost?
- 2) Does this bid include the sump pump and electrical hookup? As we've discussed, the Times will provide conduit and wiring to the area but final hookup and responsibility probably should be done by the general contractor, or sub-contractor.

As always, thanks so much for your generous help with this project.

Very truly yours,



Cooper Green

JONES & HERRIN

Architecture/Interior Design

November 3, 1995

Mr. Bob Ludwig, Publisher
The Huntsville Times
2317 Memorial Parkway SW
Huntsville, Alabama 35801

Dear Bob:

I enjoyed talking with you at the recent reception for the Russian visitors at Frank Franz's house. I was particularly pleased to learn that you are pursuing the work to preserve the 1856 Memphis & Charleston Rail Way Freight Depot. Upon inquiry in about 1980, the Smithsonian's railway expert said that he knew of no other freight depot in the United States in continuous operation since 1856. He knew of only one in the world that slightly exceeded this record, one in England built in the mid 1830's that was still operating as of about 1980.

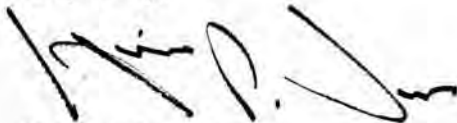
I have happily furnished a design for your loading dock that will stop the "rising damp" problem in the brick that was caused by excavating a wheel-well pit by the building and dock some years ago. The water in the pit soaks up into the porous 1856 brick, deteriorating the mortar and the brick.

I have also volunteered to train a mason to properly re-lay and re-point the loose bricks, a job not nearly so simple as it sounds. I have yet to find a mason who knows how to do it correctly. One mason I've worked with who does a good job is the Reverend Ezell Conerly, 533-6439 (home telephone).


I can also assist in roof repair techniques. The cement-fiber tiles are still available, with glass-fiber reinforcing.

"The Huntsville Times" is to be commended for your efforts to preserve and restore this nationally important historic structure. Please let me know how I can be of help.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

copy: HJ 

attachments

Feb 7 '96

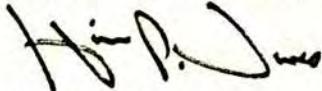
JONES & HERRIN

Architecture/Interior Design

1856 Memphis and Charleston Railroad Freight Depot

About two-hundred feet east of the 1860 Huntsville Depot Museum and on the opposite side of the tracks is the 1856 Memphis and Charleston Railroad Freight Depot, one of the most important historic railroad structures in the United States. In about 1980, the Smithsonian Institution's expert on railroading history stated that to his knowledge this was the oldest surviving freight depot in continuous operation in the United States. He knew of only one other in the world slightly older - an English depot of the 1830's. It is now 1996 and this depot is still receiving freight after one-hundred and forty years of continuous operation. Its activities are now limited to receiving newsprint rolls for "The Huntsville Times".

This unique survivor of American railroading needs to be retained and restored, and the Historic Huntsville Foundation, Norfolk Southern and The Huntsville Times are and have been working to find a good solution for all concerned.



Harvie P. Jones, FAIA

VGS

Construction Co., Inc.

2420 8TH STREET
HUNTSVILLE, AL 35805

April 17 , 1996

Jones & Herrin Architects
104 Jefferson Street
Huntsville, AL 35801

Attn: Mr. Jones
Re: Huntsville Depot Storage Building Roof Repair

Dear Harvie:

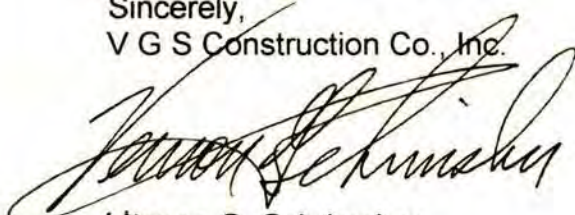
I will apply a sheet metal " patch" to the hole in the depot storage building roof for the sum of \$ 430.00.

For this amount, I will scaffold up and lay ladders on the roof to work off of while installing a sheet of 26 ga. metal over the damaged area. The sheet of metal would be caulked to the existing roof and then screwed down.

While every effort will be made not to damage the existing roof shingles, something minor may occur.

If I may be of service, please call.

Sincerely,
V G S Construction Co., Inc.



Vernon G. Schrimsher

JONES & HERRIN

Architecture/Interior Design

April 18, 1996

Mr. Ben Walker
c/o Samples Properties
655 Gallatin Street S.W.
Huntsville, Alabama 35801

Re: 1856 Huntsville Freight Depot

Dear Ben:

The Norfolk-Southern Railroad has agreed to give the 1860 Scottsboro Depot and its site to the City of Scottsboro. The Railroad initially asked \$1.5 million and the closing of two crossing streets. After discussion, the Railroad backed off. An apparent factor was the very wide right-of-way in Scottsboro (perhaps 150'-200'). The Railroad realized that the City could demand upkeep of this considerable acreage.

Their papers are not yet in hand but the Historical Association has no reason to believe the agreement won't be upheld. They dealt with a Mr. Sundquist of Knoxville, which may be in a different Railroad division from Huntsville (?). They expect a land-reversion clause to be part of the agreement.

Respectfully,



Harvie P. Jones, FAIA
HPJ/tm

copy: Ms. Diane Ellis
Ms. Carole Harless
HJ
file

JONES & HERRIN

Architecture/Interior Design

August 14, 1996

Mr. Harry Burnett
Capital Marketing
3005 L&N Drive, S. W.
Huntsville, Alabama 35801

Mr. Cutter Hughes, Attorney at Law
200 Clinton Avenue, West
Huntsville, Alabama 35801

Depot
Miss. Bldg.
Mr. Andy Kelly
United Construction of Huntsville, Inc.
906 Bob Wallace Avenue, S.W.
Huntsville, Alabama 35801

Mr. Cooper Green, General Manager
The Huntsville Times
2317 Memorial Parkway, S.W.
Huntsville, Alabama 35801

Re: 1856 Memphis & Charleston Railroad Freight Depot Restoration

Gentlemen:

The Historic Huntsville Foundation appreciates your interest in the preservation and restoration of the oldest continuously operating freight depot in the United States, the 1856 Memphis & Charleston Depot in Huntsville.

Attached is a rough site plan of 1990 which indicates that the gross enclosed area of the building is about 5,400 square feet (verify) and shows that about one-hundred cars could be parked in the area south of the yard tracks over to the east end of the depot, without removing any siding tracks.

We recently restored a long-neglected and partly abandoned 1902 auditorium at Judson College. The total restoration cost was \$62.68 per square foot including all new mechanical, electrical, plumbing, elevator, chair-lift, and extensive repairs.

The 1860 Memphis & Charleston Passenger Depot was in much worse shape in 1977 than the Freight Depot and required much elaborate millwork and finish replication and repair, plus an elevator, fire stair, plumbing, mechanical and electrical. The cost in 1980 was \$51.74 per square foot, which inflates to \$87.44 in 1996 per "R.S. Means" cost data.

In view of the above and the more than seven-hundred restorations this office has executed it is my opinion that the 1856 Freight Depot could have its exterior (only) restored for about \$30-\$40 per square foot, and be restored inside and out for about \$80-\$90 per square foot total.

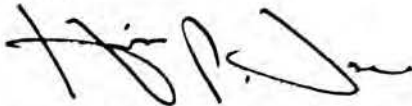
The Historic Huntsville Foundation has in 1993 donated \$2,500 to patch the roof and will in 1996 donate \$450 to patch the remaining hole on the south slope.

It is important to remove the pond at the west end of the building which has caused the mortar deterioration at this location.

I agree with Andy Kelly's idea of fully restoring the exterior as a minimum first effort to preserve the building and make it more appealing to a possible tenant. If this is not financially possible at this time, then we should drain the pond, properly restore the masonry deterioration, and complete the roof patching to stabilize and preserve the building until funds and a use for full restoration come along. We should not let this nationally important structure get away.

When the building is officially in City hands it should be nominated to the National Register of Historic Places, for which it is eminently eligible. I have all the material needed to prepare this and will be happy to donate this effort. Please let me know when this is appropriate. I also serve on the Alabama Historical Commission National Register Review Board.

Respectfully,

A handwritten signature in black ink, appearing to read "Harvie P. Jones". The signature is fluid and cursive, with a large initial "H" and "J".

Harvie P. Jones, FAIA
HPJ/tm

copy: Ms. Diane Ellis, Huntsville Historic Foundation
HJ

attachments

JONES & HERRIN

Architecture/Interior Design

August 22, 1996

Mr. Vernon Schrimsher
VGS Construction Company
2420 8th Street
Huntsville, Alabama 35805

Re: 1856 Huntsville Freight Depot

Dear Vernon:

The Historic Huntsville Foundation has asked me to have you proceed with the temporary patching of the hole in the south slope of the 1856 depot roof per your quote of \$430 of April 17, 1996.

Padded walking supports such as ladders or large planks or plywood sheets should be used to avoid breaking the brittle cement-fiber shingles, and for safety since there is some degree of decay in the decking.

I notice that some of the circa 1993 metal roof patches have blown loose. Please let me know a cost for re-securing these and I'll try to get the Historic Huntsville Foundation to approve their reattachment.

Thanks!



Harvie P. Jones, FAIA
HPJ/tm

copy: Ms. Diane Ellis
HJ

CERTIFICATE OF INSURANCE | 8/30/1996

PRODUCER
Fountain & Parker Ins
 P. O. Box 2320
 Huntsville, AL 35804-2320
 (205) 551-0060

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. IT DOES NOT AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW.

INSURED
VGS CONSTRUCTION COMPANY, INC.
 2420 8TH STREET
 HUNTSVILLE, AL 35805

COMPANIES AFFORDING COVERAGE
 COMPANY A: **STATE AUTO**
 COMPANY B: **ALABAMA HOME BUILDERS SIF**
 COMPANY C:
 COMPANY D:
 COMPANY E:

COVERAGES

This is to certify that policies of insurance listed below have been issued to the insured named above for the policy period indicated, notwithstanding any requirement, term or condition of any contract or other document with respect to which this certificate may be issued or may pertain, the insurance afforded by the policies described herein is subject to all the terms, exclusions and conditions of such policies. Limits shown may have been reduced by paid claims.

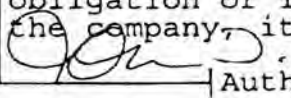
CO INSURANCE POLICY NUMBER DATES

A	GENERAL LIABILITY <input checked="" type="checkbox"/> Gen Liability <input checked="" type="checkbox"/> Occ <input type="checkbox"/> CM <input type="checkbox"/> OCP <input type="checkbox"/>	PBP0019649	Effective 06/08/96 Expiration 06/08/97	\$ 1,000,000 General Agg \$ 1,000,000 Prod/CoOps Agg \$ 500,000 Pers/Adv Inj \$ 500,000 Occurrence \$ 50,000 Fire Damage \$ 5,000 Medical Exp
A	AUTO LIABILITY <input checked="" type="checkbox"/> Any Auto <input type="checkbox"/> All Owned <input type="checkbox"/> Scheduled <input checked="" type="checkbox"/> Hired <input checked="" type="checkbox"/> Non-owned <input type="checkbox"/> Garage Liab <input type="checkbox"/>	BAP0072350	Effective 06/08/96 Expiration 06/08/97	\$ 1,000,000 CSL BI (person) BI (accident) PD
	EXCESS LIABILITY <input type="checkbox"/> Umbrella <input type="checkbox"/> Other		/ / / /	\$ Occurrence \$ Aggregate
B	WORKERS COMP AND EMPLOYERS LIAB	00200HBA977	01/01/96 01/01/97	[] Statutory Lmts \$ 100,000 Each Accident \$ 500,000 Disease-Limit \$ 100,000 Disease-Empl
			/ / / /	\$ \$

Description of operations/locations/vehicles/other
 FAX: 534-2289

CERTIFICATE HOLDER

HUNTSVILLE HISTORICAL FOUNDATION
 MR. HARVIE JONES/JONES & HERRIN
 104 JEFFERSON ST
 HUNTSVILLE, AL 35804

CANCELLATION
 Should any of the above described policies be cancelled before the expiration date thereof, the issuing company will endeavor to mail 10 days written notice to the certificate holder named to the left, but failure to mail such notice shall pose no obligation or liability of any kind upon the company, its agents or reps.

 Authorized Representative

JONES & HERRIN

Architecture/Interior Design

Aug. 30 '96 Harvie P. Jones, FAIA

Mrs Diane Ellis
2004 Brandy Circle
35811

re- Hist. Hr. Ford
Daget roof patching

Dear Diane —

Attached is a copy of
VGS Const's
Certif. of Insur.
dated 8/30/96.

An insurance person can
give an opinion as to
the adequacy of this
coverage.

Herrin

104 Jefferson Street
Huntsville, Alabama 35801
Telephone 205/539-0764
Fax 205/534-2289

C-HJ



EXIST. DOCK TOP

NOTE!

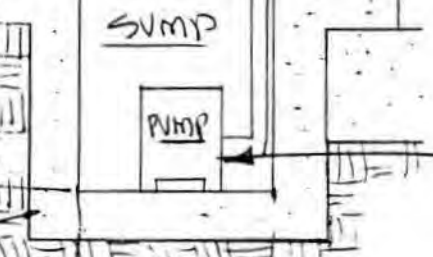
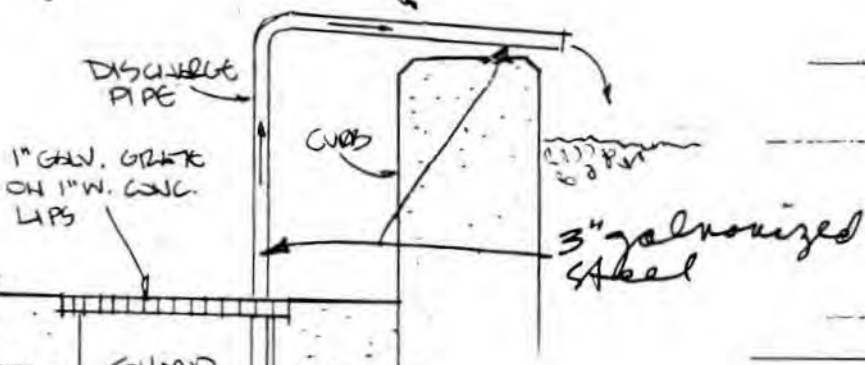
ELECT. POWER RUN TO SUMP
BY OWNER.
HOOKUP BY THIS CONTR.

SPILL ON
WEST SIDE
OF CURB

1856 MEHT
(OLDEST)

4'-3" AT SUMP ONLY

Min
Depth
3'-0"



SUMP CAN BE A P.C. CONCRETE PIPE
OR POURED IN PLACE (WHICH OPTION)
SIZE TO FIT PUMP ETC

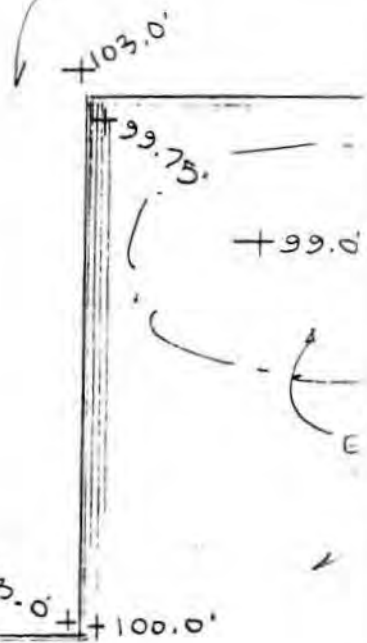
SUMP & PUMP

SEE PLAN FOR LOC.

Min
2'4"

Weil Pump Co
Model 2435, 1/2 hp
w/ differential float
switch, 100 GPM
Minimum capacity
208 or 230 FREIGHT
DEPOT
phase

EXIST. CONCR. DOG

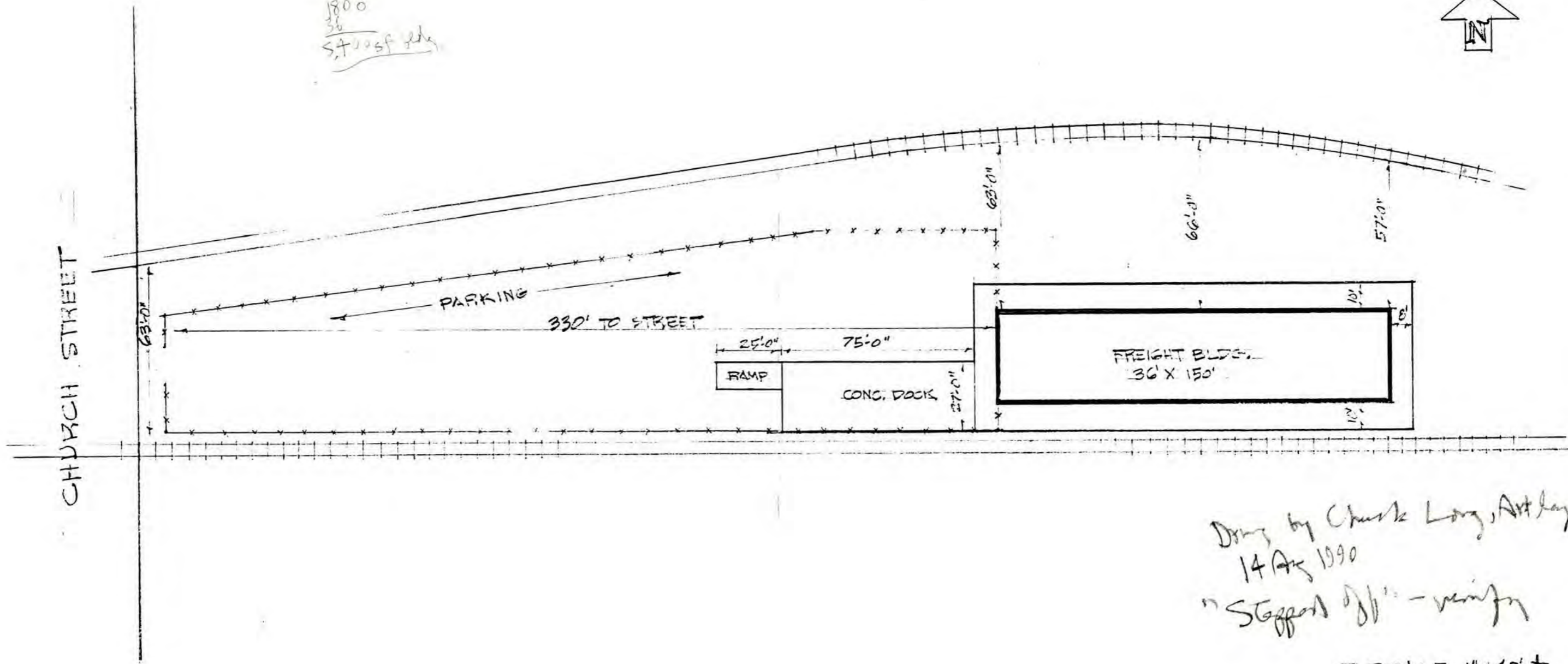


103.0' + 100.0'

166
 56 x
 996
 830
 9256
 - 5400
 3896 sf lot

 36
 150
 1800
 36
 5,100 sf site

Proj = 10,000 sf ±



Drawn by Chuck Long, Art Layer
 14 Aug 1990
 "Stepped off" - verify

SCALE 1" = 40' ±

HUNTSVILLE NEWS

Viewpoints

William C. Green, Jr., *Publisher*
Lee Woodward, *Editor and General Manager*
David Bowman, *Editorial page editor*

A-4

Friday, January 21, 1994

A plea for the depot

Efforts are underway to save the old freight depot across the tracks from the splendidly restored Huntsville passenger depot.

Principals in this effort are largely coming from stalwarts in the Huntsville Historic Foundation, which has a remarkable track record to date, including the Harrison Brothers Hardware store on the Court Square; the Rogers house, that was moved to be part of Alabama's Constitution Village; and many other landmarks in the Twickenham and Old Town historic districts.

Credit should also go to Rep. Bud Cramer's office, particularly to the congressman's district representative, Lynne Berry Lowery, who is leaving her post this month after doing innumerable good service for constituents in the Fifth Congressional District. She has been working with the freight depot owners, Norfolk-Southern, and their responses so far have been quite encouraging.

Preservation architect Harvie Jones and others would probably not find any disagreement in our assertion that this antebellum structure is the most significant industrial building surviving in Huntsville, but not yet "saved", for another century or so of new life.

We lost the Dallas Mill by fire and the Merrimack Mill by demolition. We earnestly hope that Huntsville will come together to help save this depot.

It was built for the Memphis & Charleston Rail Road about 1856 and not too long ago was apparently the oldest such depot still in service in the United States.

An informal presentation was made to the Huntsville Depot's "Friends of the Depot" advisory group yesterday afternoon. We hope that body and others will be able to find the ways-and-means to make this possible.

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ViewpointsBob Ludwig, *Publisher*Lee Woodward, *Editor and General Manager*David Bowman, *Editorial Page Editor*

A-4

Thursday, June 16, 1994

Hobbs Island gets a study

The House of Representatives approved the Appropriations Subcommittee on Energy and Water Development's bill Tuesday by a vote of 393-29.

Included are several interesting provisions for North Alabama:

- \$150,000 for a feasibility study on creating an environmental park at Hobbs Island on Huntsville's southernmost river-edge.

- \$1.8 million for the U.S. Department of the Interior to establish a national wildlife refuge at Key Cave in Lauderdale County.

- \$100,000 for a study of flooding conditions at Huntsville Spring Branch and \$150,000 for an ongoing flood study at Muscle Shoals.

- \$100,000 for historic preservation projects in the Shoals area and in Huntsville. This will involve historic recording of several sites by the Historic American Building Survey and Historic American Engineering Record, including "Wilson and Wheeler dams, a TVA nitrate plant in Muscle Shoals, a stone arch railroad bridge in Huntsville, and the Huntsville Railroad Depot." (1956 freight bldg.)

The study concerning the future of Hobbs Island, a 340-acre breadloaf in the Tennessee River at Huntsville's southernmost edge, is particularly intriguing.

For one thing, the study meshes with a concept plan, prepared by LDR Associates in 1991, for the Ditto Landing recreation complex. Here are a few highlights from that Ditto Landing plan:

"The entire 340-acre island is the setting for a proposed wildlife and nature preserve, and Native American historic park / preserve.

"Its purpose will be to retain the natural environment and encourage wildlife, and to enhance the archaeological themes that enrich the island. Access to the island will be via the pedestrian bridge near the golf course, and in a later phase, from a second pedestrian bridge to be located in the vicinity of the current ferry. The island's existing wooded edges will be preserved and enhanced with the addition of riprap edges and re-vegetation.

"The nature preserve will encompass almost all of the 340-acre island. Ponds

current agricultural land to be excavated as fill for the new golf course.

"Nature trails, picnicking facilities, and bird-watching blinds will be randomly situated to take advantage of the island's scenic overlooks and viewpoints. In order to minimize impact, pedestrian access trails will be limited to viewing spots.

"The island's second preserve will be the Native American Indian park and nature preserve, which will serve to enhance the archaeological aspects of the native Cherokee and Chickasaw Indians.

"Although this preserve is concentrated in the southern tip of the island, it is likely that archaeological digs will occur throughout the island as part of the island's redevelopment into a nature park."

With the exception of the marina facilities at Ditto Landing, Huntsville and Madison County have done relatively little with its Tennessee River frontage. Part of the reason, of course, is that a good chunk is part of the 38,000-acre Redstone Arsenal military reservation and the rest is under the "management" of the Tennessee Valley Authority, which has thousands of miles of shoreline in its care.

But this study should be significant, one way or the other, particularly if the major archaeological sites on the island are to be dealt with in a sensible way.

We appreciate the active participation of Rep. Bud Cramer's office in getting these environmental and cultural-resource appropriations through all the legislative hurdles.

After some decades of relative indifference to enhancing our environmental quality of life, we are returning to a "do the right thing" attitude. We should credit organizations like the Huntsville Land Trust, which has saved big chunks of Monte Sano, the city's scenic backdrop, and is encouraging better stewardship of our most valuable natural acreage. City and county environmental planning has developed access points to Flint River and has created the new Aldrich Creek Greenway, between Mountain Gap and Green Cove Road. This prototype of many such greenways will be given a ceremonial ribbon-cutting on Sunday,



DEC. '80

↑ EAST AND NORTH FACE

→ N

H50 Freight Bldg.
Huntsville Depot.
Huntsville, Ala.
December, 1980

(photos
Ralph Allen
AIA)



→ N

photos
1979
H Jones



DEC. '80

↑ SOUTH FACE LOOKING WEST

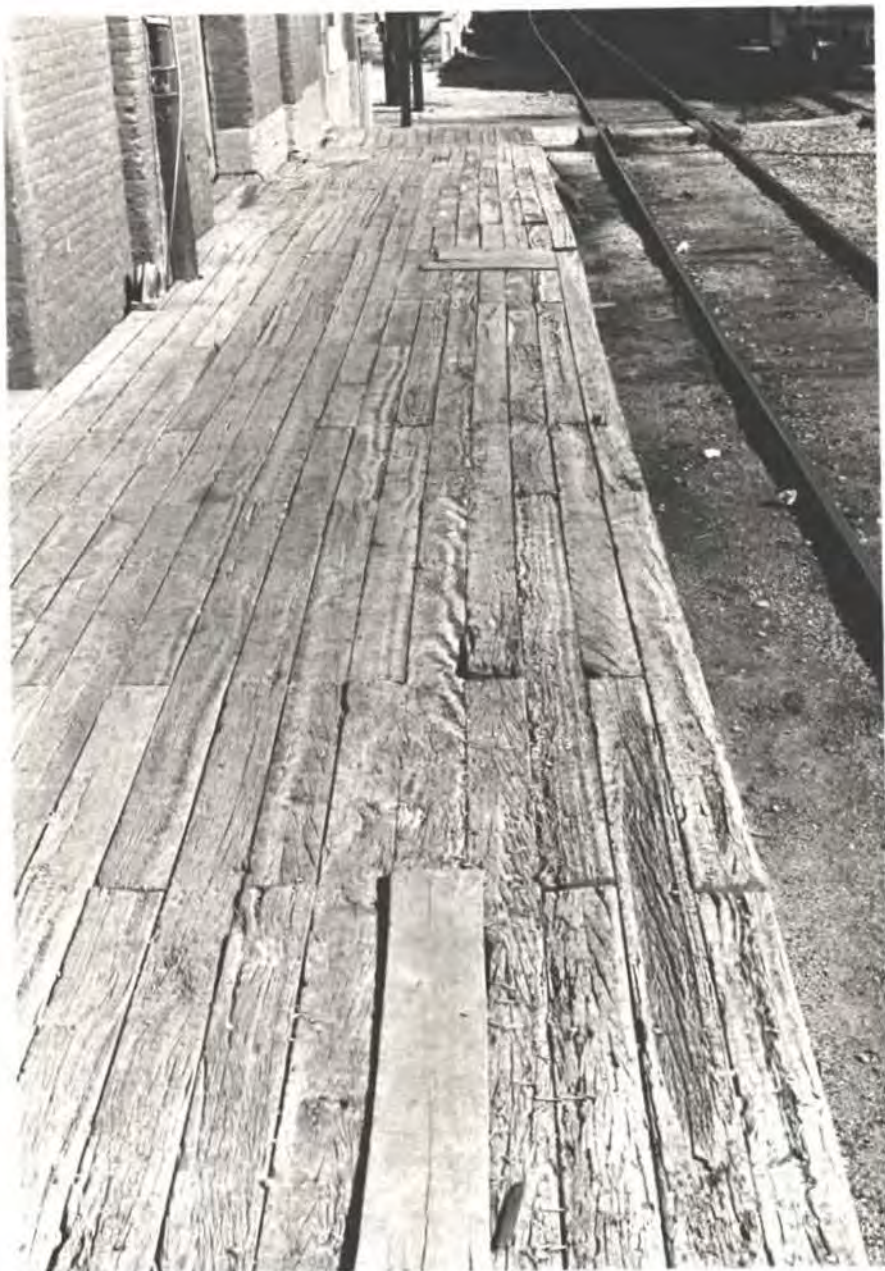


2 DEC. '80



DEC. '80

↑ SOUTH FACE LOOKING WEST



DEC. '80

↑ PLATFORM - SOUTH FACE.



DEC. '80

↑ DOOR ADDITION - SOUTH ELEVATION



DEC. '80

↑ SLIDING DOOR @ WOOD PLATFORM
SOUTH ELEVATION



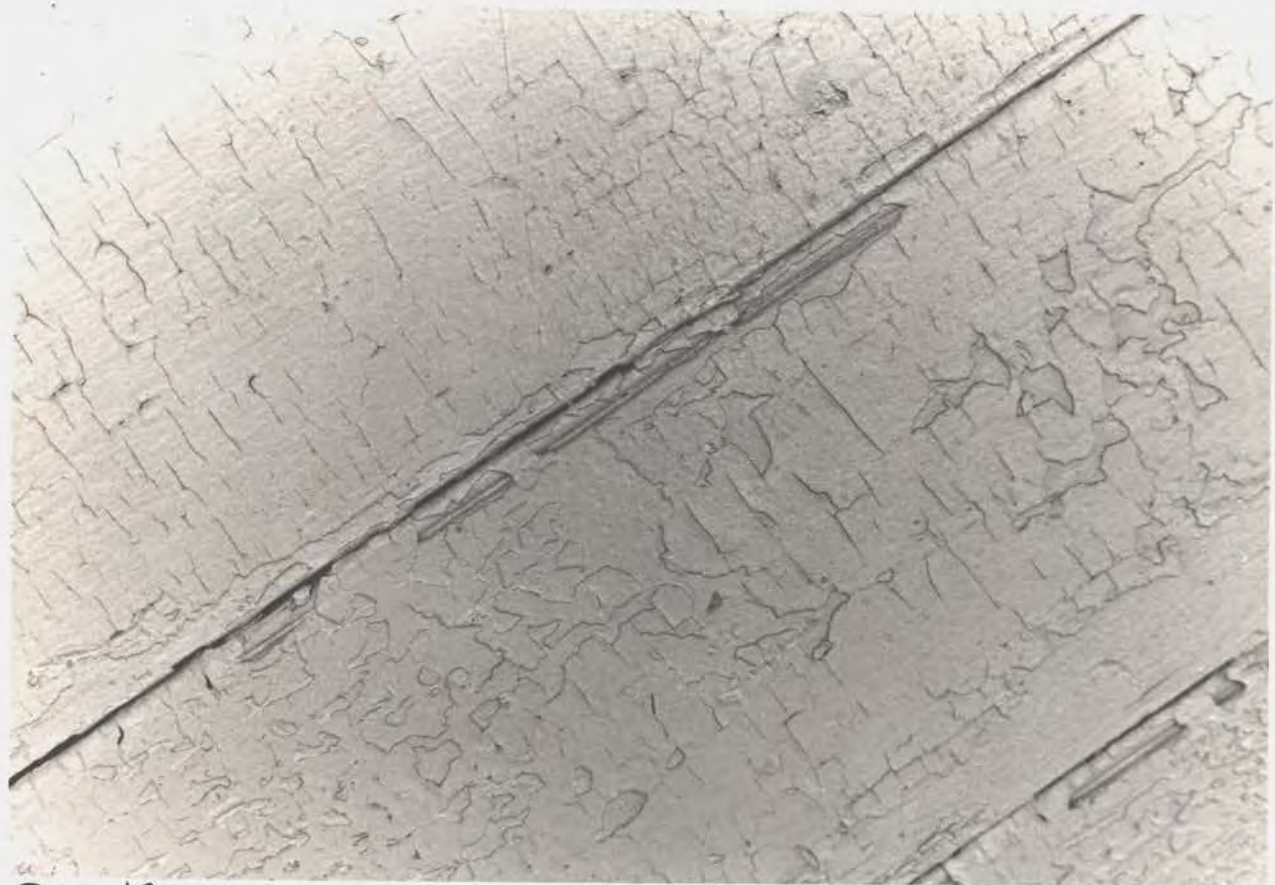
DEC. 180

↑ SLIDING DOOR DET.
SOUTH ELEVATION



DEC. '80

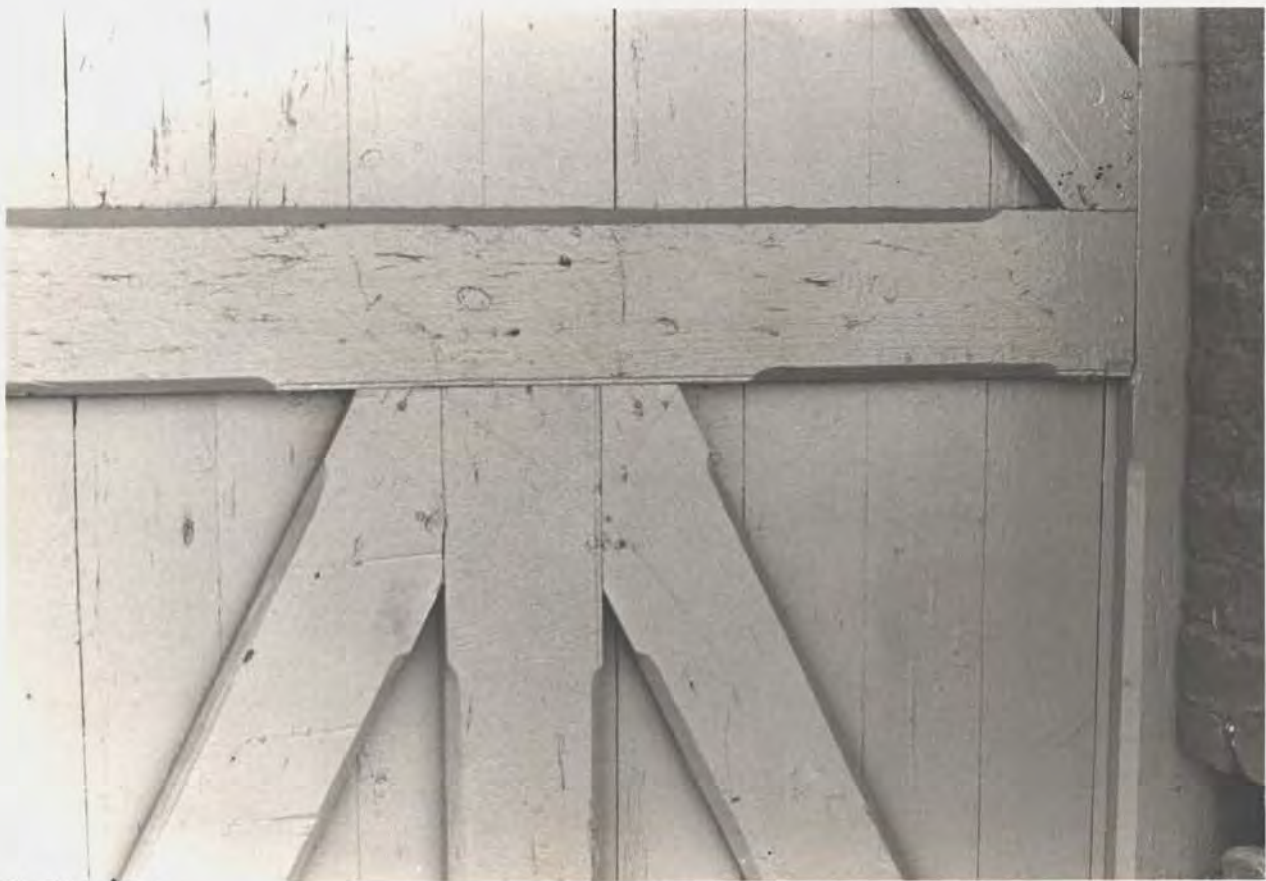
↑ SLIDING DOORS , NORTH ELEV.



DEC. '80

↑ SLIDING DOOR DET. NORTH ELEVATION

↓ SLIDING DOOR DET. SOUTH ELEVATION † NORTH



DEC. '80



DEC. '80

↑ EAST SIDE LOOKING NORTH



DEC. '80

↑ GABLE DETAILS, EAST ELEVATION



DEC. '80



DEC. '80

↑ GABLE DET. WEST ELEV.
↓



12 DEC. '80



DEC. '80

↑ ROOF overhang/canopy det.
Support brackets added.



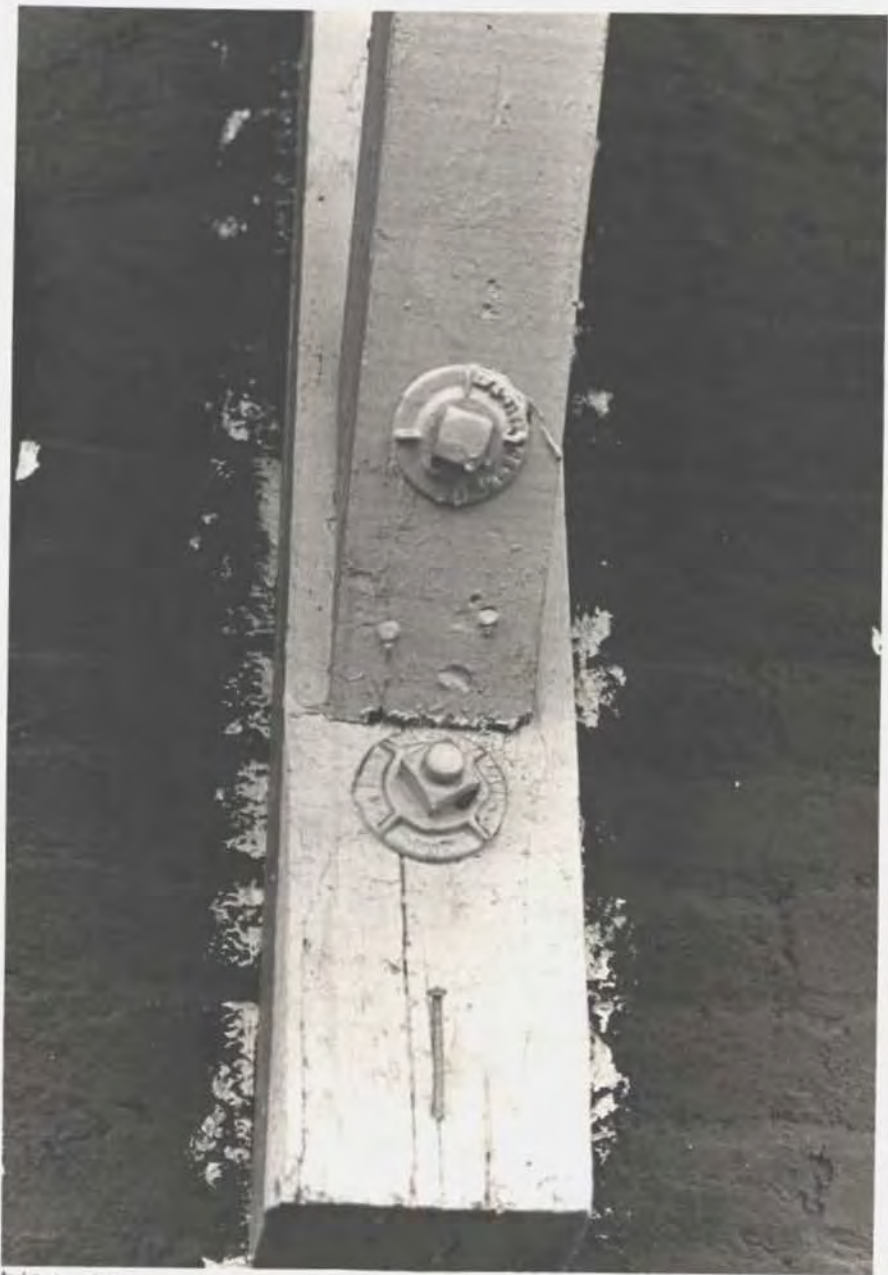
DEC 80

↑ Eave det.

↓ Eave det w/ added support bracket.

DEC 80





DET. 80

↑ BRACKET DET. - ADDED.



DEC. '80

↑ DOWN SPOUT, SOUTH ELEV.



DEC 40

↑ DOWNSPOUT DET. - BELOW WOOD PLATFORM



↑ EAST WALL



↑ FLUE DET. @ EAST WALL



↑ EAST WALL



↑ EAST / SOUTH WALLS



↑ SOUTH WALL



↑ WEST WALL
↓





↑ NORTH WALL



↑ NORTH WALL

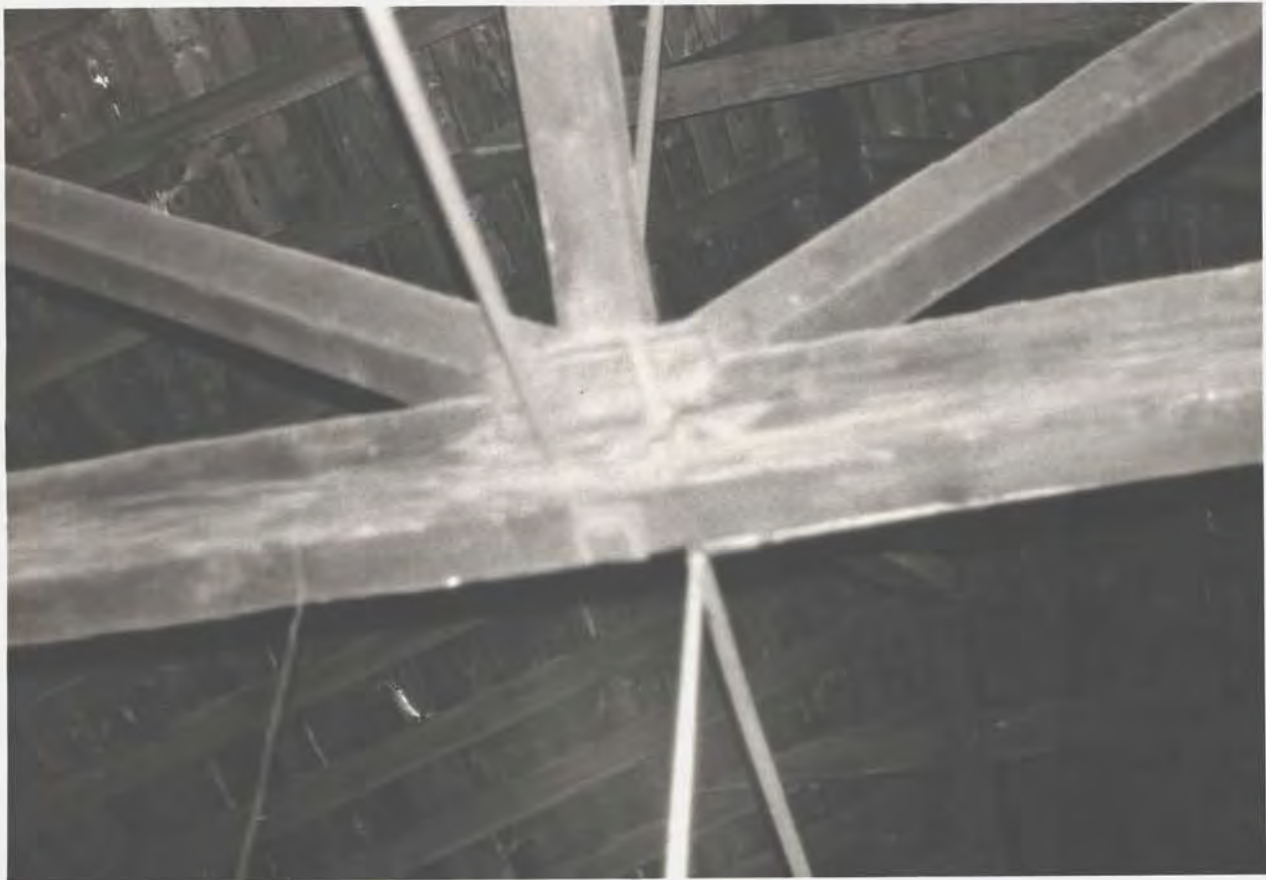


↑
↓
WOOD TRUSS DETAILS

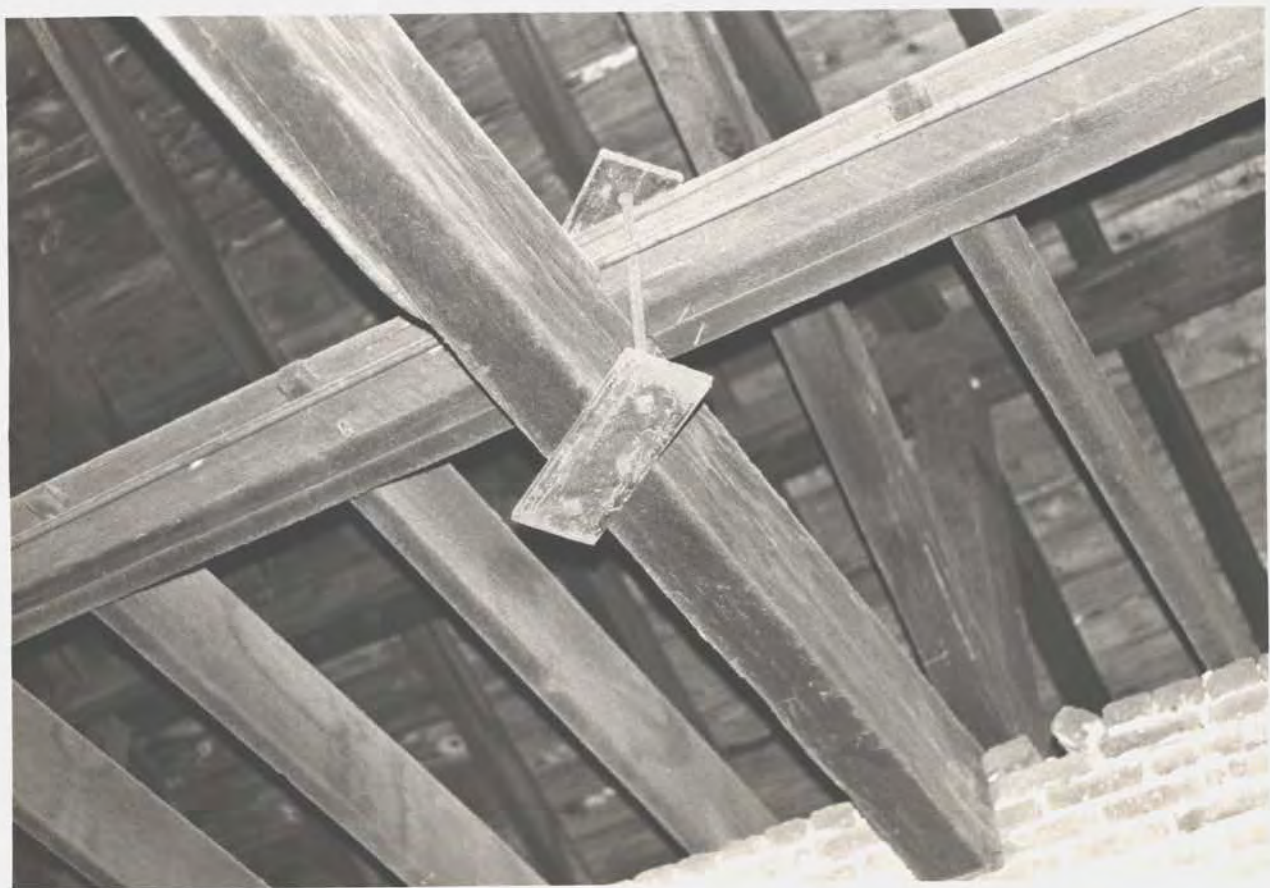


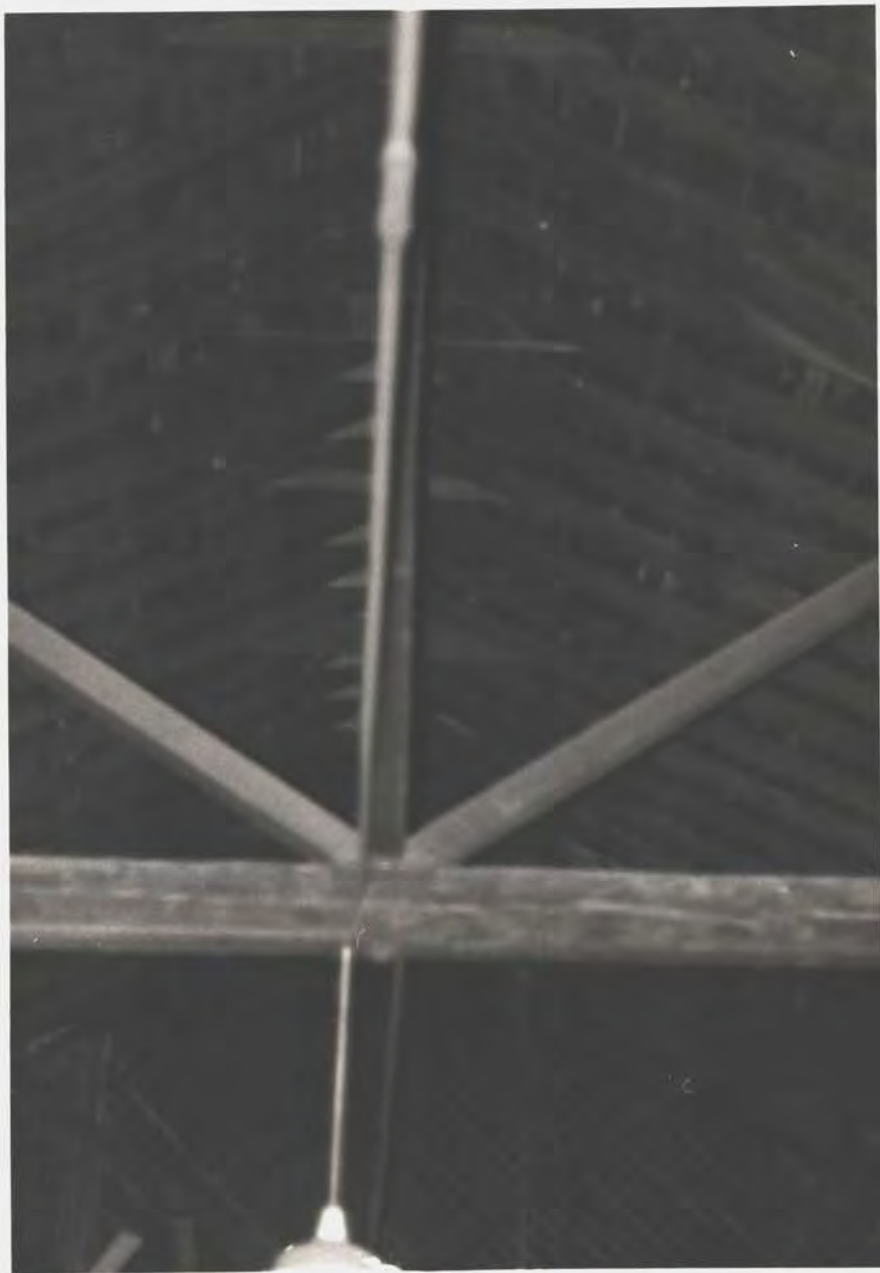


↑ WOOD TRUSS DETAILS



↑ WOOD TRUSS DETAILS
↓





↑ WOOD TRUSS DETAILS



↑ WOOD TRUSS DETAILS.





↑ WOOD TRUSS DETAILS



↑ WOOD TRUSS DETAILS
↓ BRICK PIER





↑ BRICK PIER



↑ SLIDING DOOR TRACK
↓





?



4

3

2

1



?

↑ BRICK PATTERN



↑ WOOD SLIDING DOOR DETAIL - BEADED ED'S.

↓ SLIDING DOOR STOP





↑ HARDWARE DETAIL



↑ HARDWARE DETAIL
↓



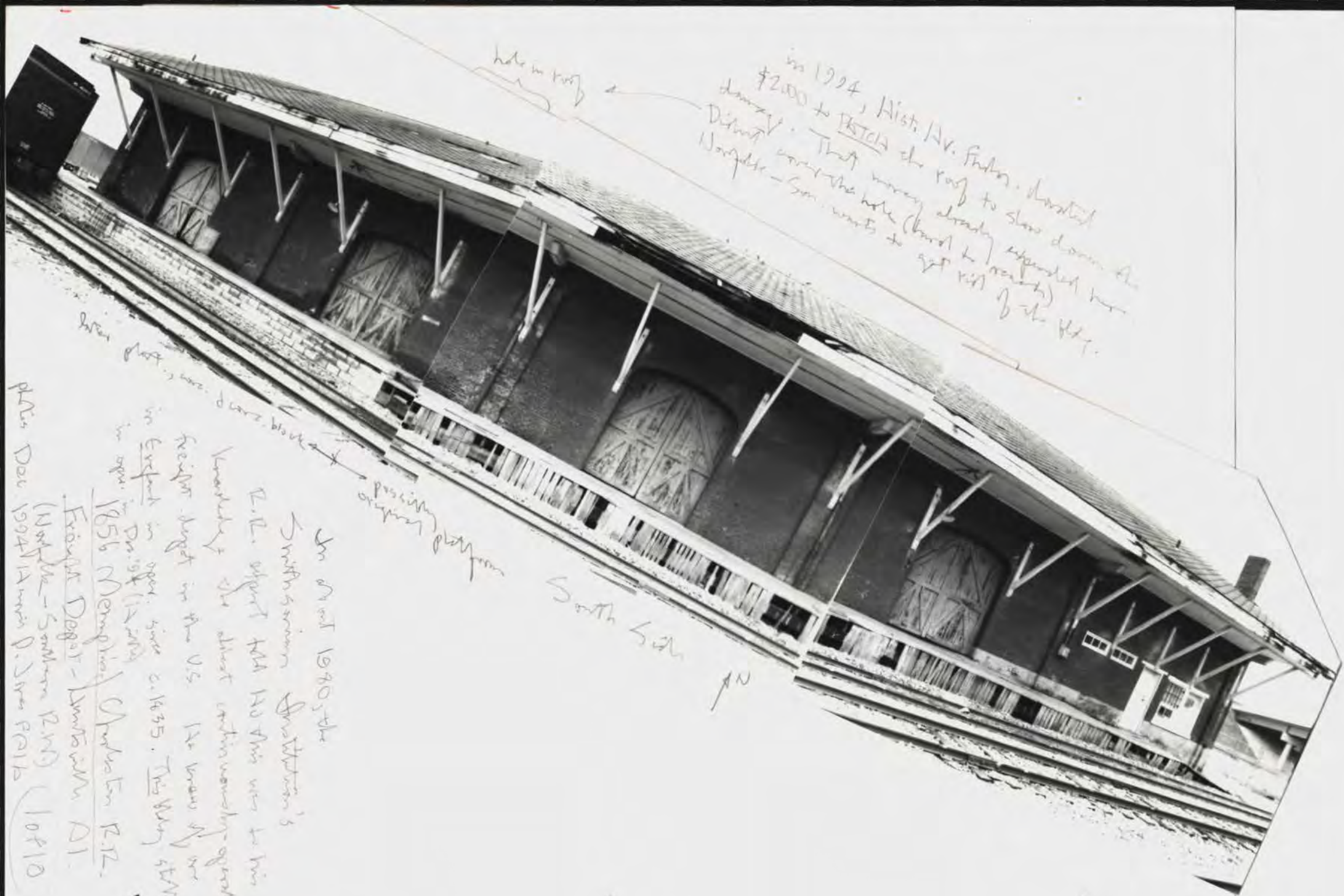


↑ HARDWARE DETAIL



↑ WOOD FLOOR DETAILS
↓





in 1924, Hist. Dr. Fisher donated
 \$2,000 to PTOLA the roof to slow down. A
 downy. That money already expended here
 District. That money already expended here
 Norfolk - Son wants to get rid of the floor.

hole in roof

possibly
 position
 looking

South side

↑

in 1940, the
 Smithsonian Institution's
 R.R. report had had this was to his
 knowledge, the about cartography operation
 freight target in the US. The name of the
 in England in open since c. 1835. The way of
 in open in 1912
 1856 Memphis, Arkansas R.R.
 Freight Depot - Dunbar, AL
 (North Me - Southern R.R.) 10810
 Plans Dec 1924 (Ames D. Jones PA12)



→ N

↑ office



→ N

↑ Wd. photo very old, perhaps 1856 (?)



2010

→ N



→ N

↑ appears orig. platform



rising damp damage

to N S.W. Door



H ←

↑ pond

← this pond has caused the rising-damp damage to the brick facade at the west end of the bldg.



A
 Paving - damage
 damage at west end

H.V. Lines

S.W.
 Corner



4010



NORTH
SIDE



one
post =
20th c.

Roof =
con. - asbestos
strips,
early 20th c.

← rising damp
barrier

()

9510



PH 111
Office at post office of Holy



West Wall



S.W. corner



Off. West wall



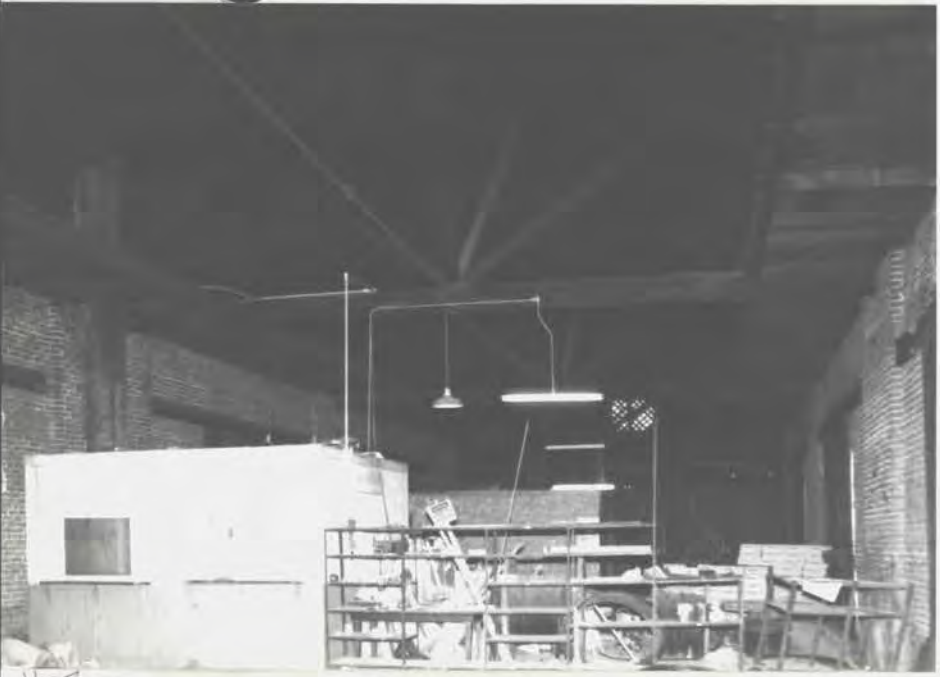
door to freight rm



West Wall



8410



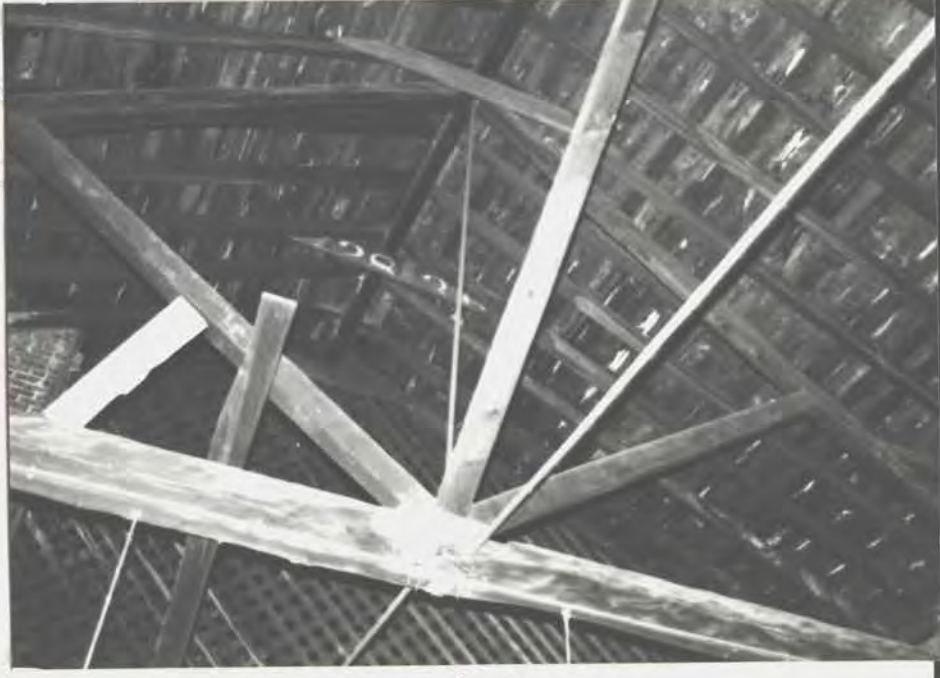
N
↑



→ W



W
→



Detail at office west wall (above)



No. Door w/ 1980's door chopped into it

SW
S.D. 




W 



door keep, C.I., probably original (?)

10 of 10



↑
← A
cor. pl

Unsuccessful attempts
to "fix" rising damp.
The way to "fix" it
was to get rid of
the pond.

Probably due to rising damp damage to wd. joints

pine pl., very thick (3 3/4"?)

