

Names:

Army Air Corps
Officials

Bell, Robert K.

Chamber of
Commerce

Places:

Huntsville, AL

Types:

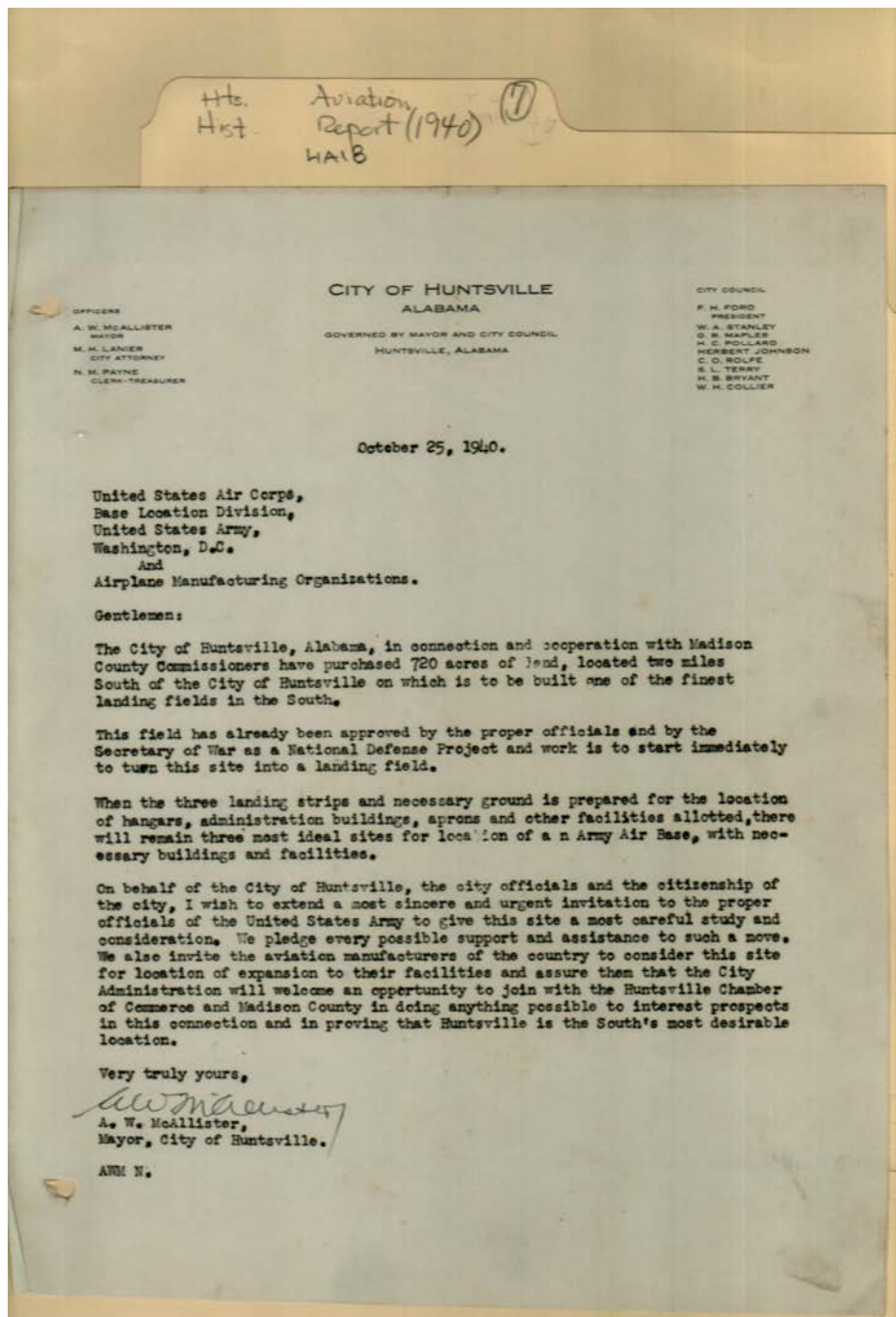
correspondence

Dates:

Oct 25, 1940

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7
Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 2 r04a18-07-000-0067 [Contents](#) [Index](#) [About](#)



Names:

McAllister, A. W.,
Mayor

Places:

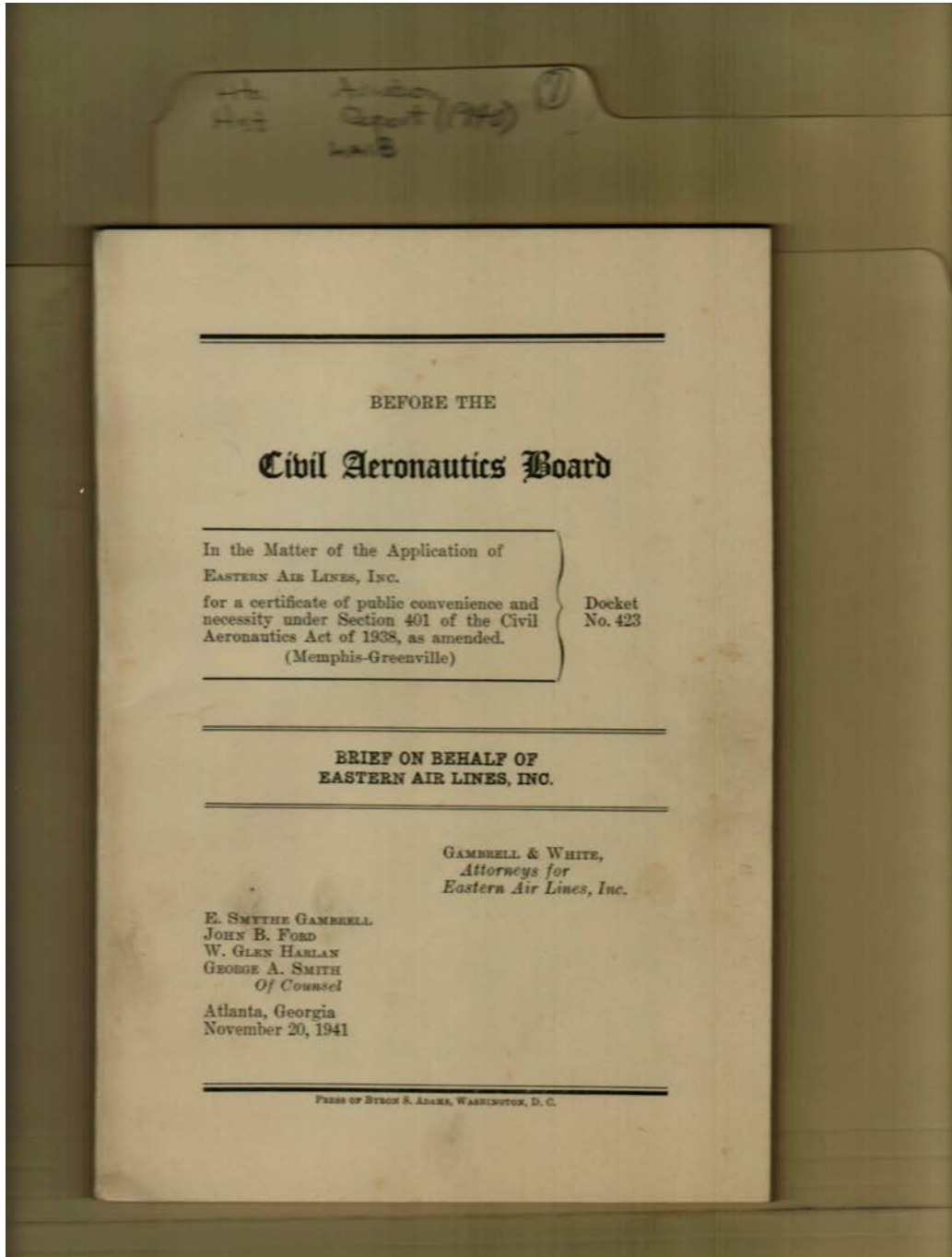
Huntsville, AL

Types:

correspondence

Dates:

Oct 25, 1940



Names:

Civil Aeronautics
Board

Eastern Air Lines,
Inc.

Ford, John B.
Gambrell, E. Smythe

Harlan, W. Glen
Smith, George A.

Places:

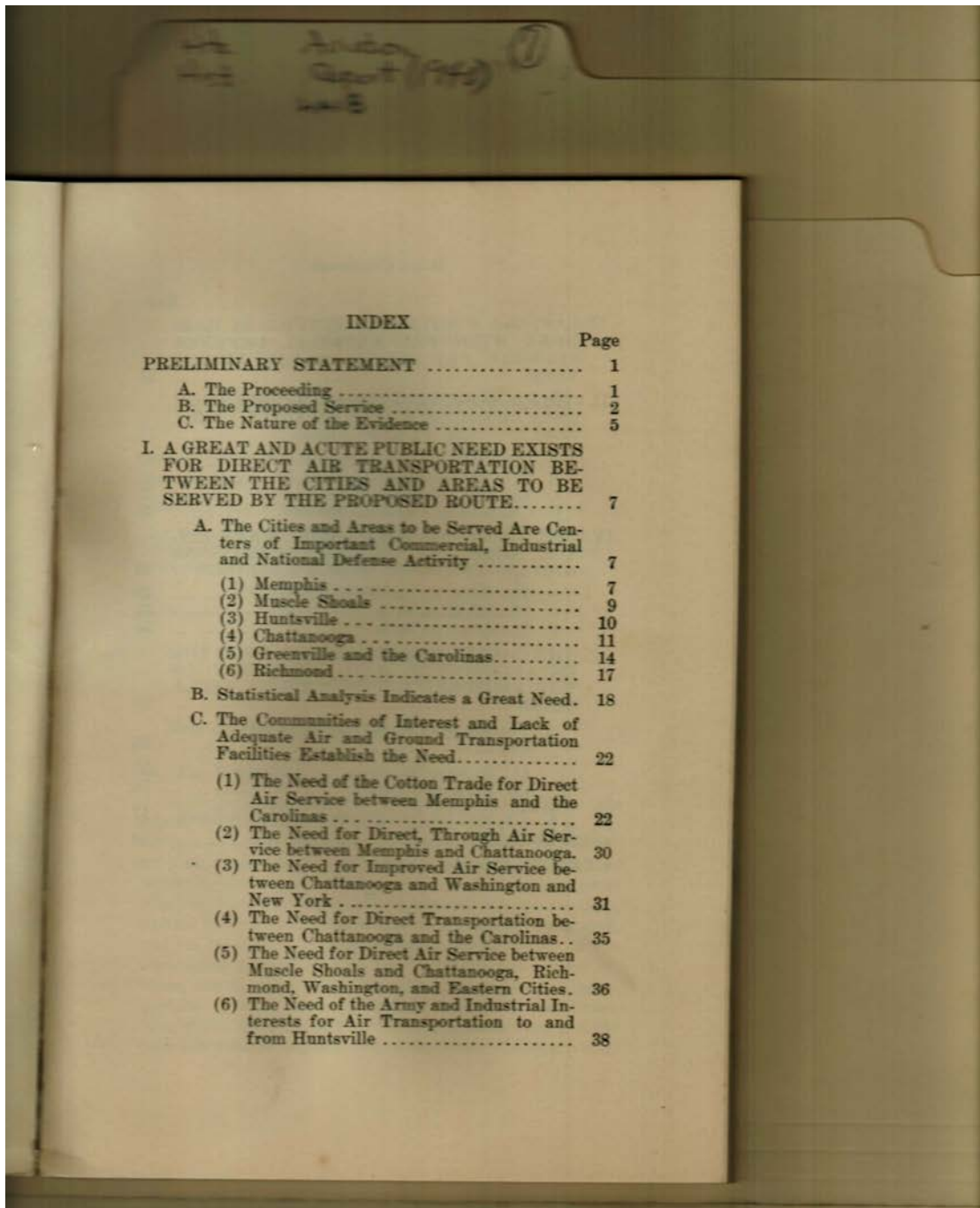
Atlanta, GA

Types:

brief

Dates:

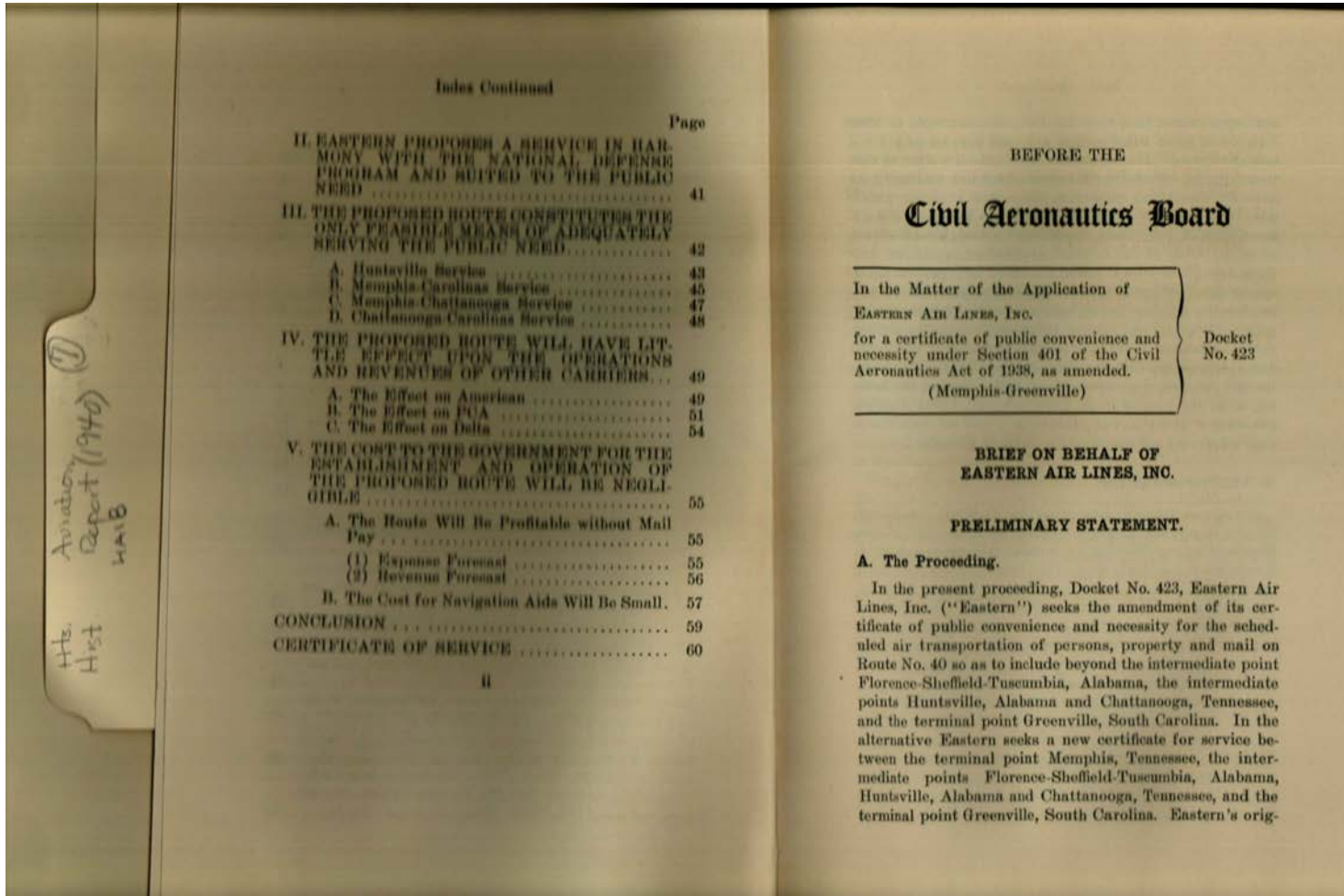
Nov 20, 1941



INDEX		Page
PRELIMINARY STATEMENT		1
A. The Proceeding		1
B. The Proposed Service		2
C. The Nature of the Evidence		5
I. A GREAT AND ACUTE PUBLIC NEED EXISTS FOR DIRECT AIR TRANSPORTATION BETWEEN THE CITIES AND AREAS TO BE SERVED BY THE PROPOSED ROUTE.....		7
A. The Cities and Areas to be Served Are Centers of Important Commercial, Industrial and National Defense Activity		7
(1) Memphis		7
(2) Muscle Shoals		9
(3) Huntsville		10
(4) Chattanooga		11
(5) Greenville and the Carolinas.....		14
(6) Richmond		17
B. Statistical Analysis Indicates a Great Need.		18
C. The Communities of Interest and Lack of Adequate Air and Ground Transportation Facilities Establish the Need.....		22
(1) The Need of the Cotton Trade for Direct Air Service between Memphis and the Carolinas		22
(2) The Need for Direct, Through Air Service between Memphis and Chattanooga.		30
(3) The Need for Improved Air Service between Chattanooga and Washington and New York		31
(4) The Need for Direct Transportation between Chattanooga and the Carolinas..		35
(5) The Need for Direct Air Service between Muscle Shoals and Chattanooga, Richmond, Washington, and Eastern Cities.		36
(6) The Need of the Army and Industrial Interests for Air Transportation to and from Huntsville		38

Names:
Index

Types:
study



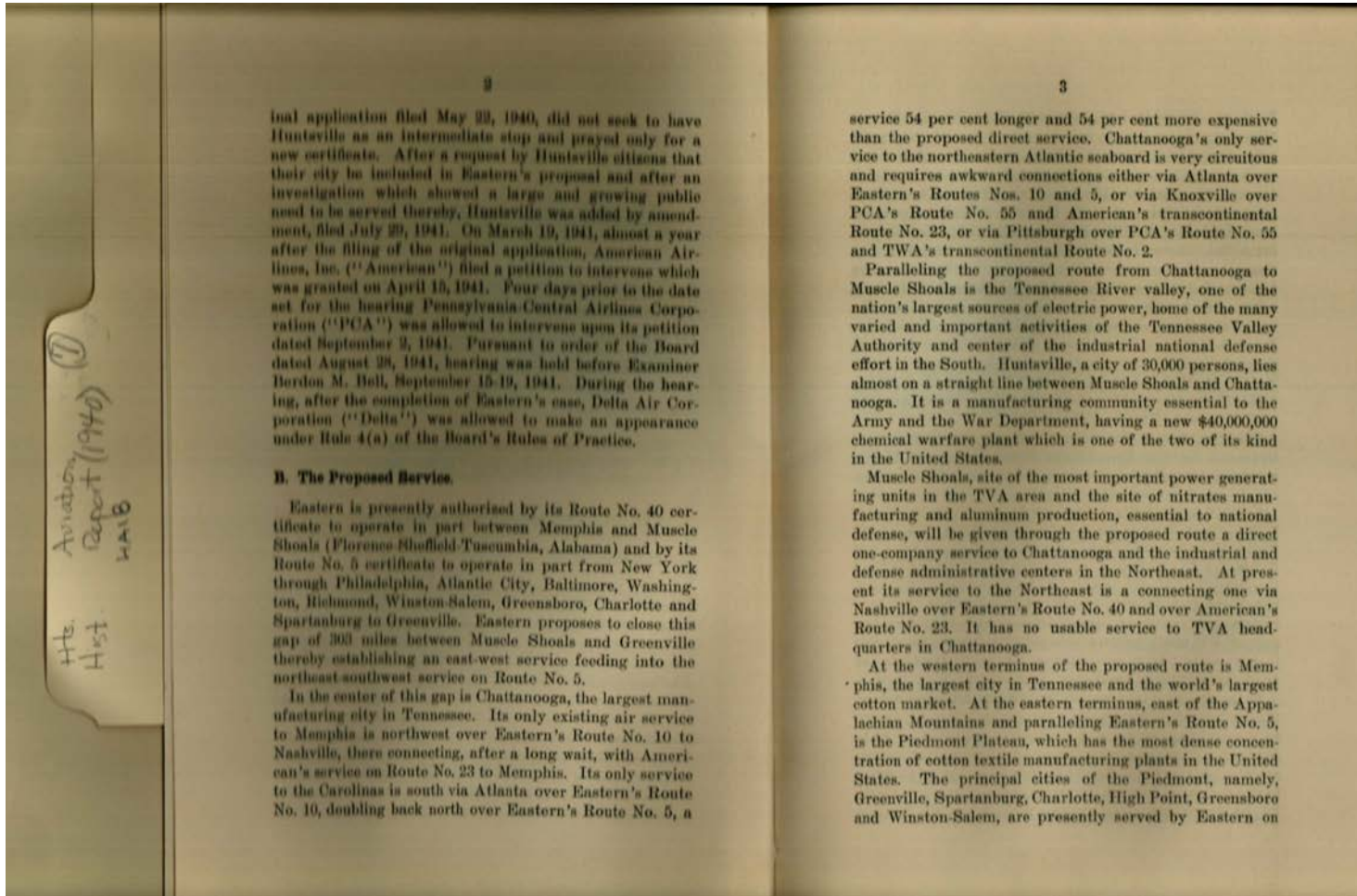
Names:

Civil Aeronautics Board

Eastern Air Lines, Inc.

Types:

book

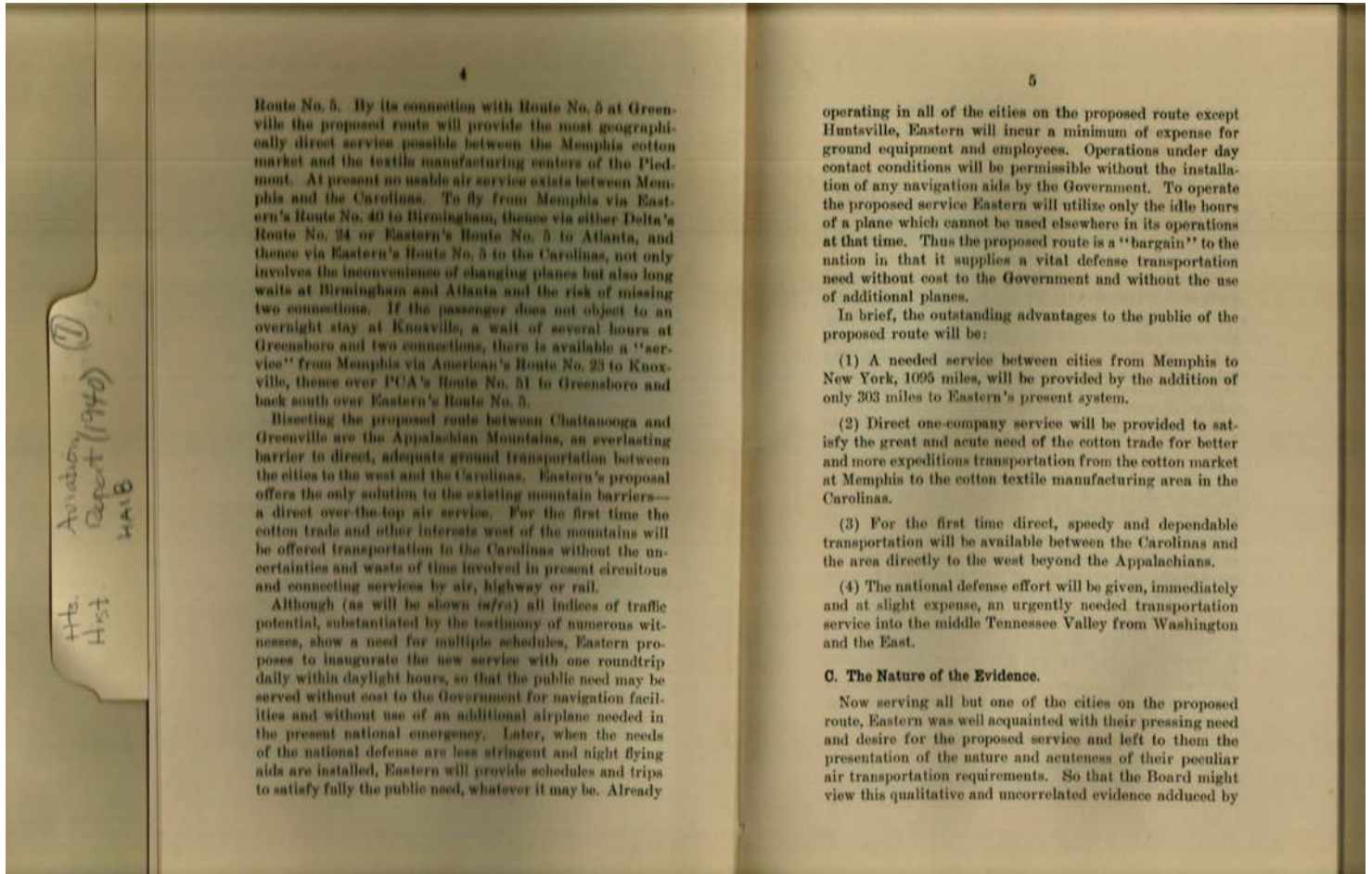


Names:

Proposed Service

Types:

book

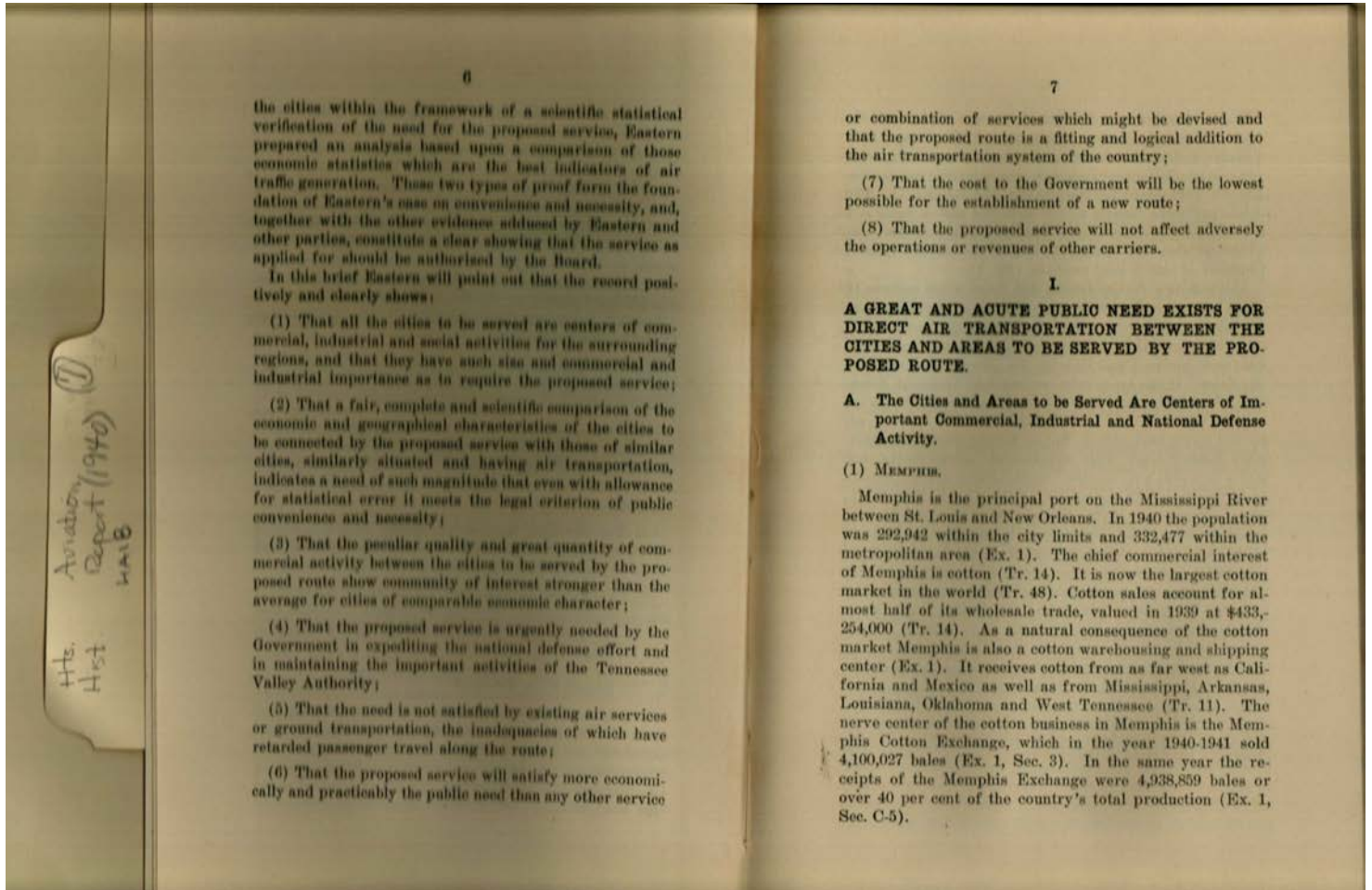


Names:

Evidence

Types:

book

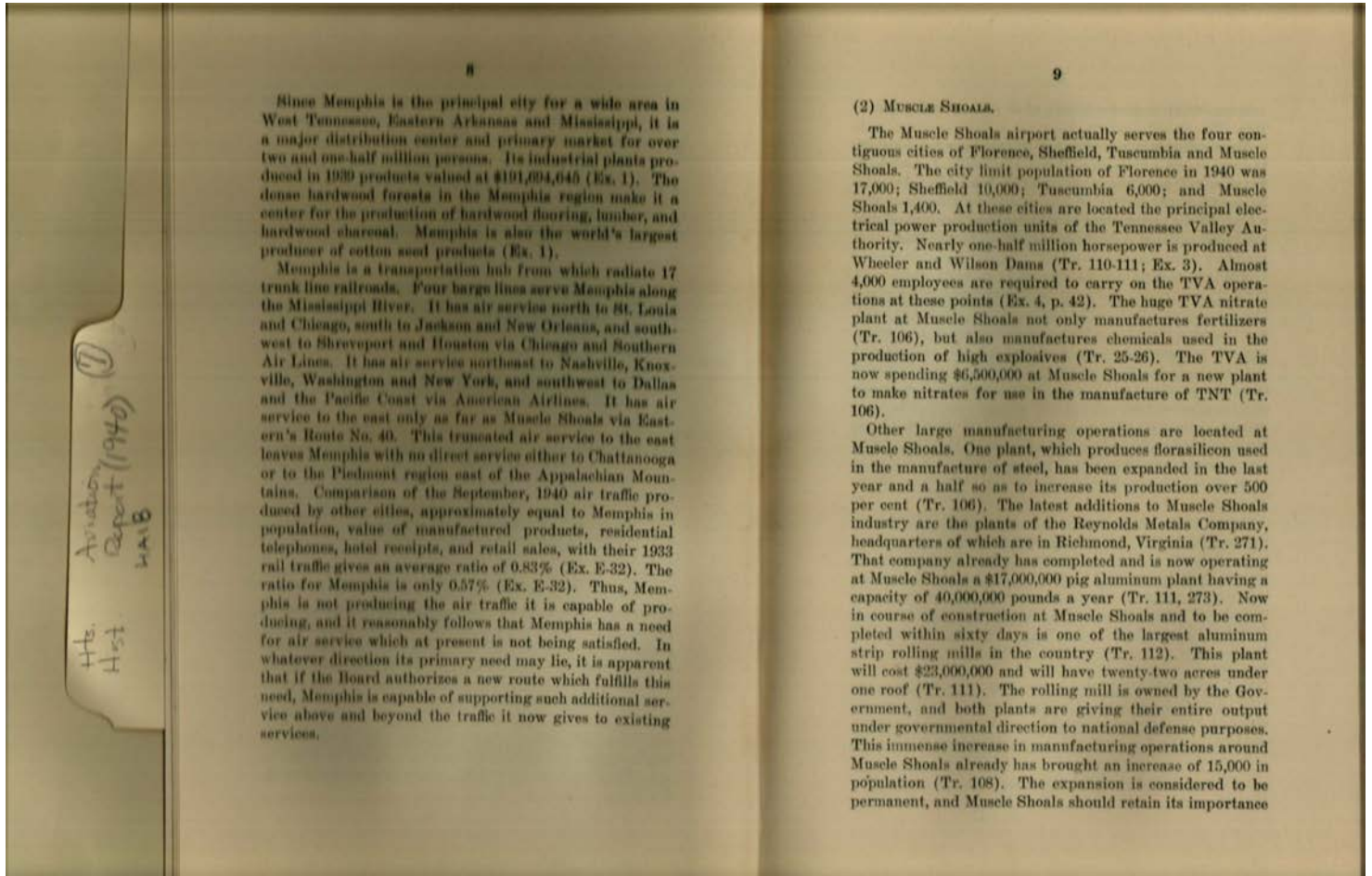


Names:

Public Need for Air
Transportation

Types:

book

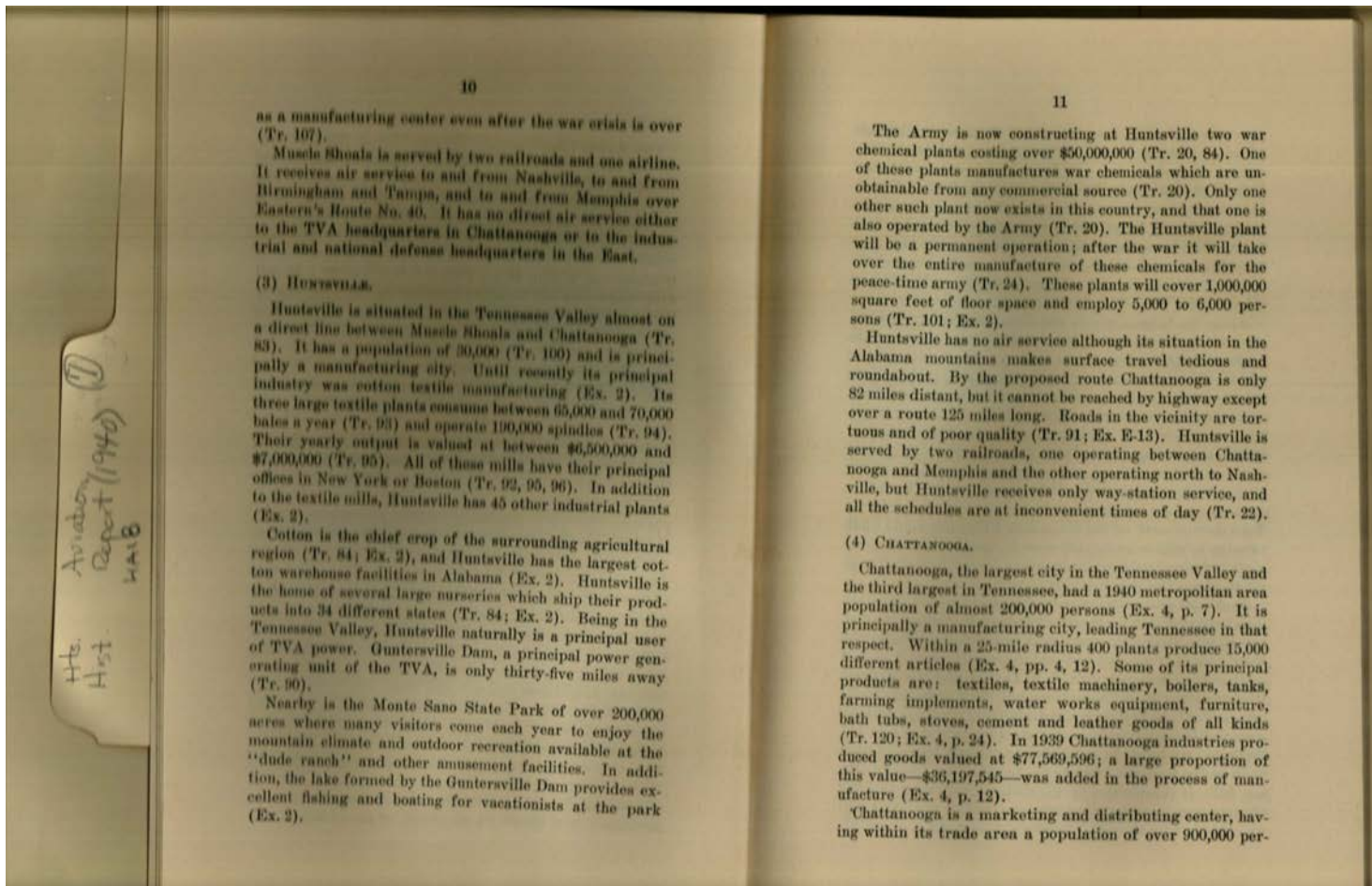


Names:

Memphis to Muscle
Shoals

Types:

book



Names:

Chattanooga Location

Huntsville Location

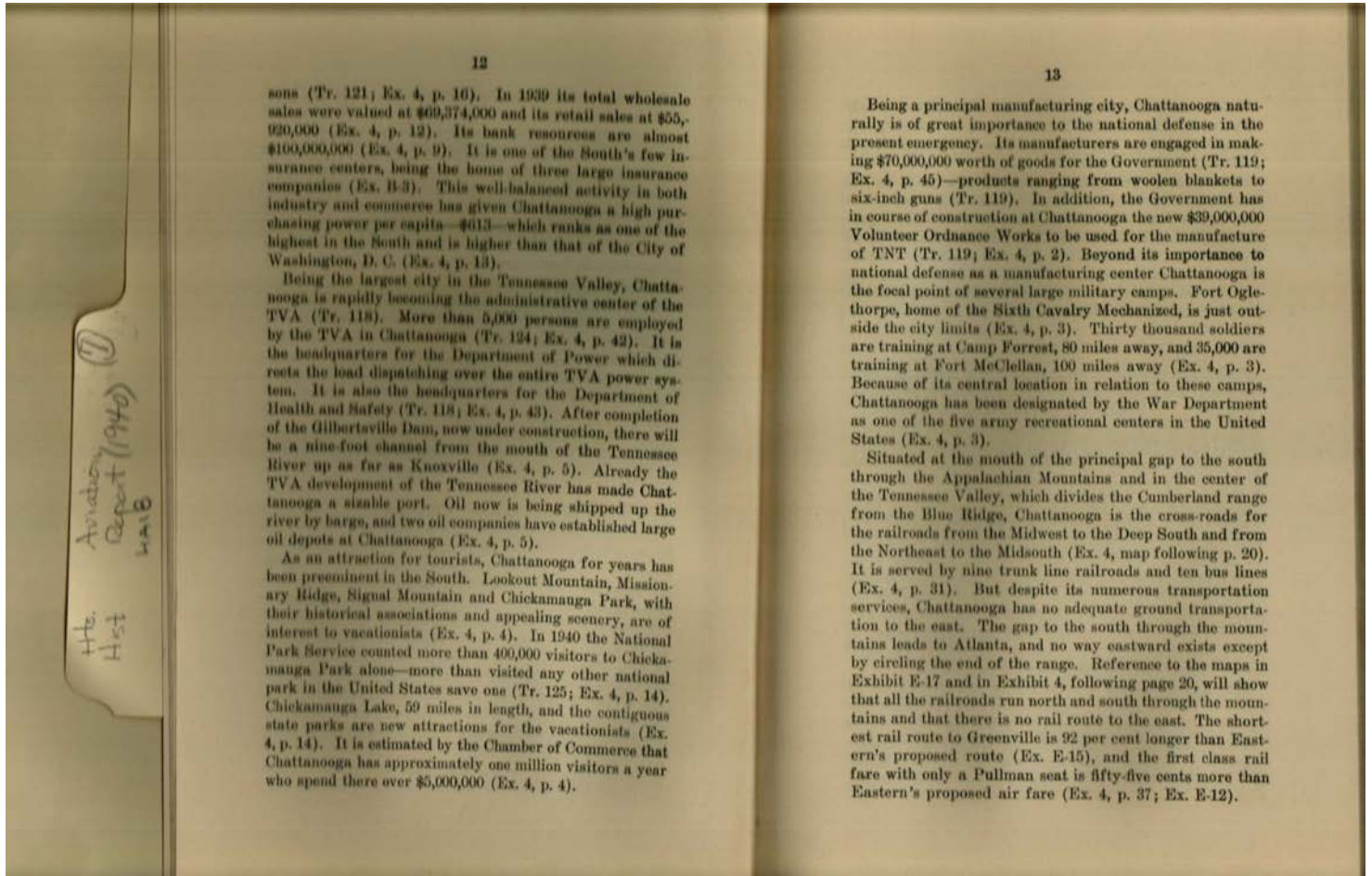
Types:

book

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7

Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 11 r04a18-07-000-0076 [Contents](#) [Index](#) [About](#)



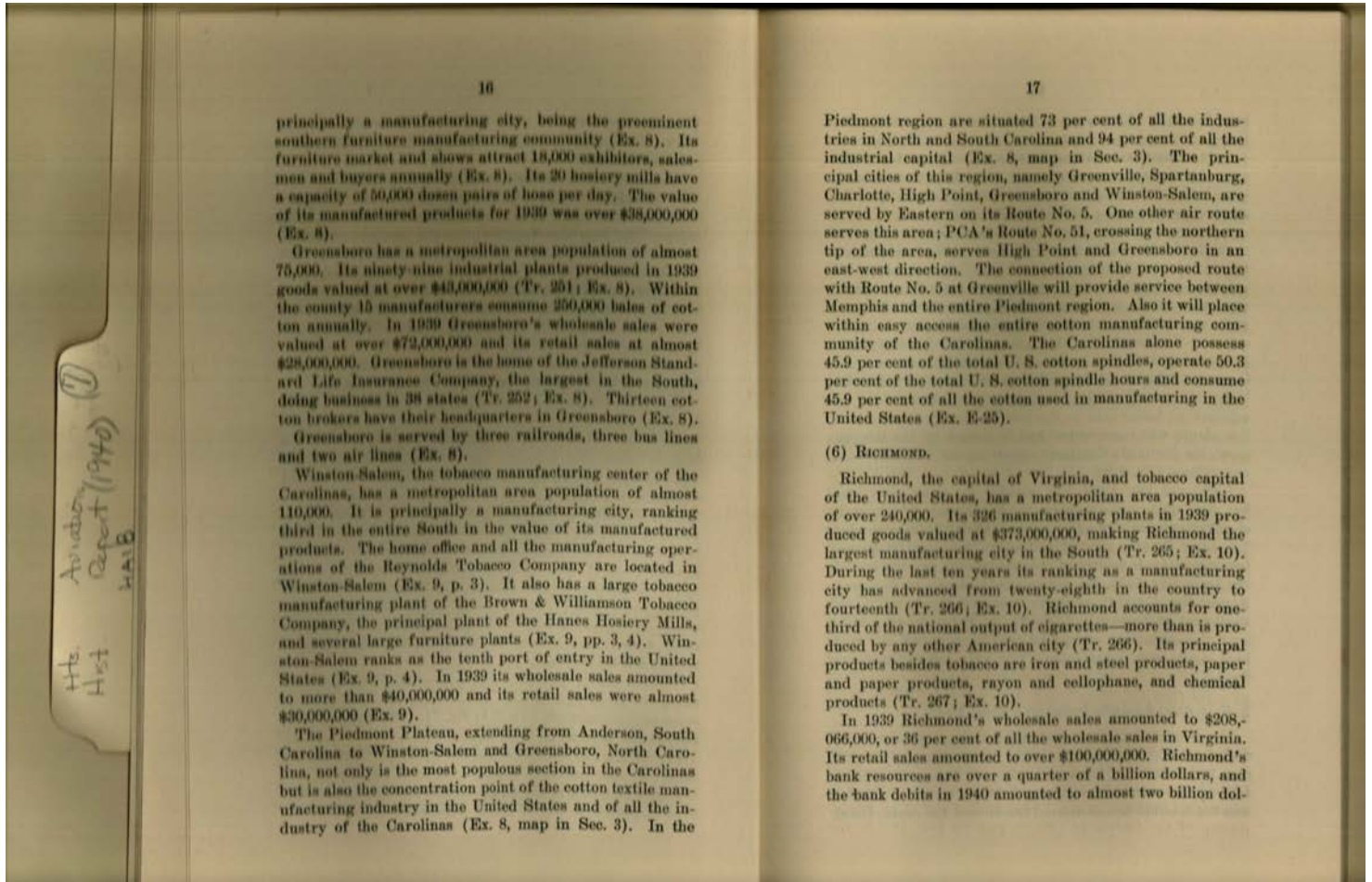
Names:

Chattanooga is center

TN Valley

Types:

book

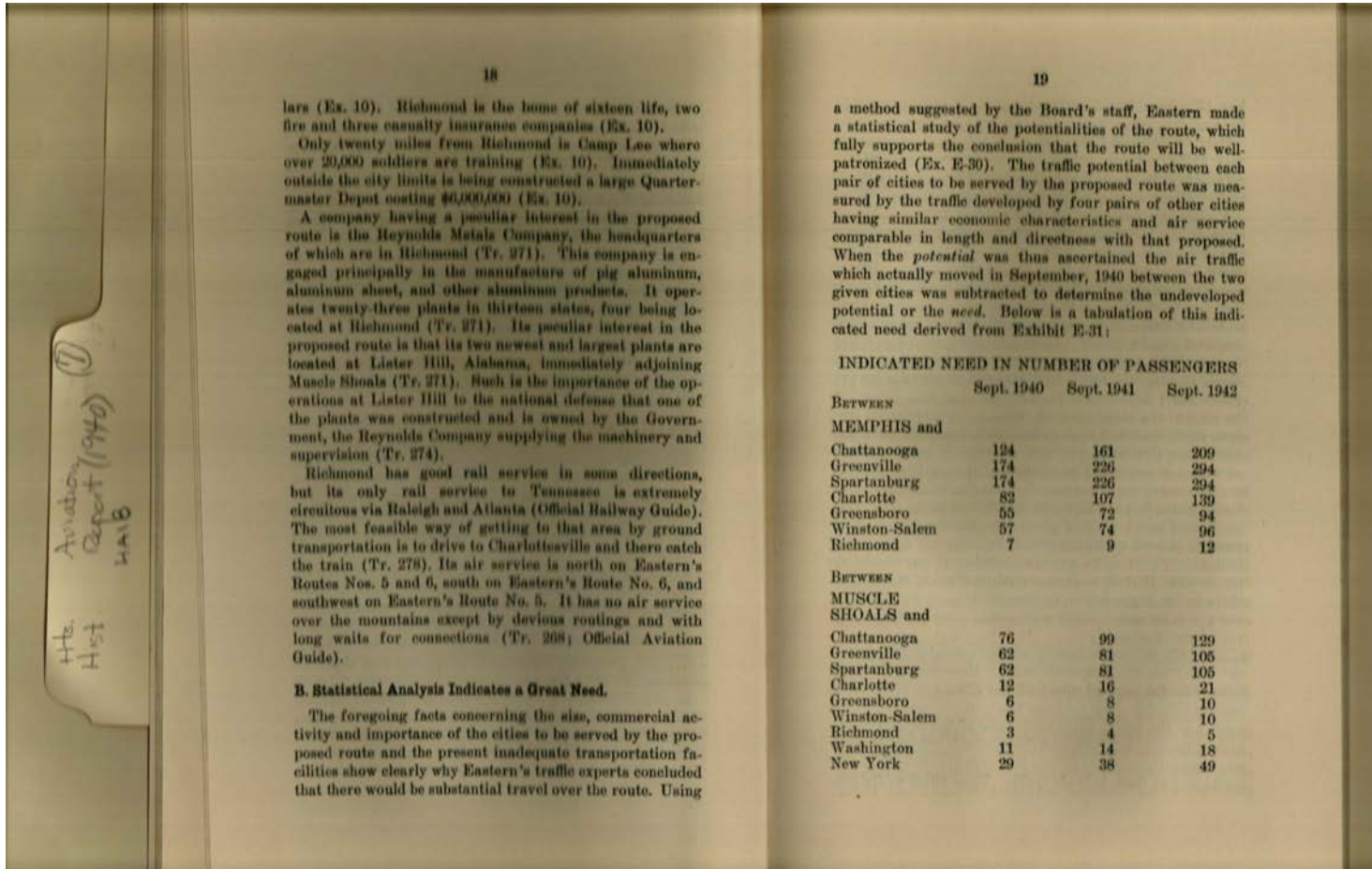


Names:

South Carolina needs

Types:

book

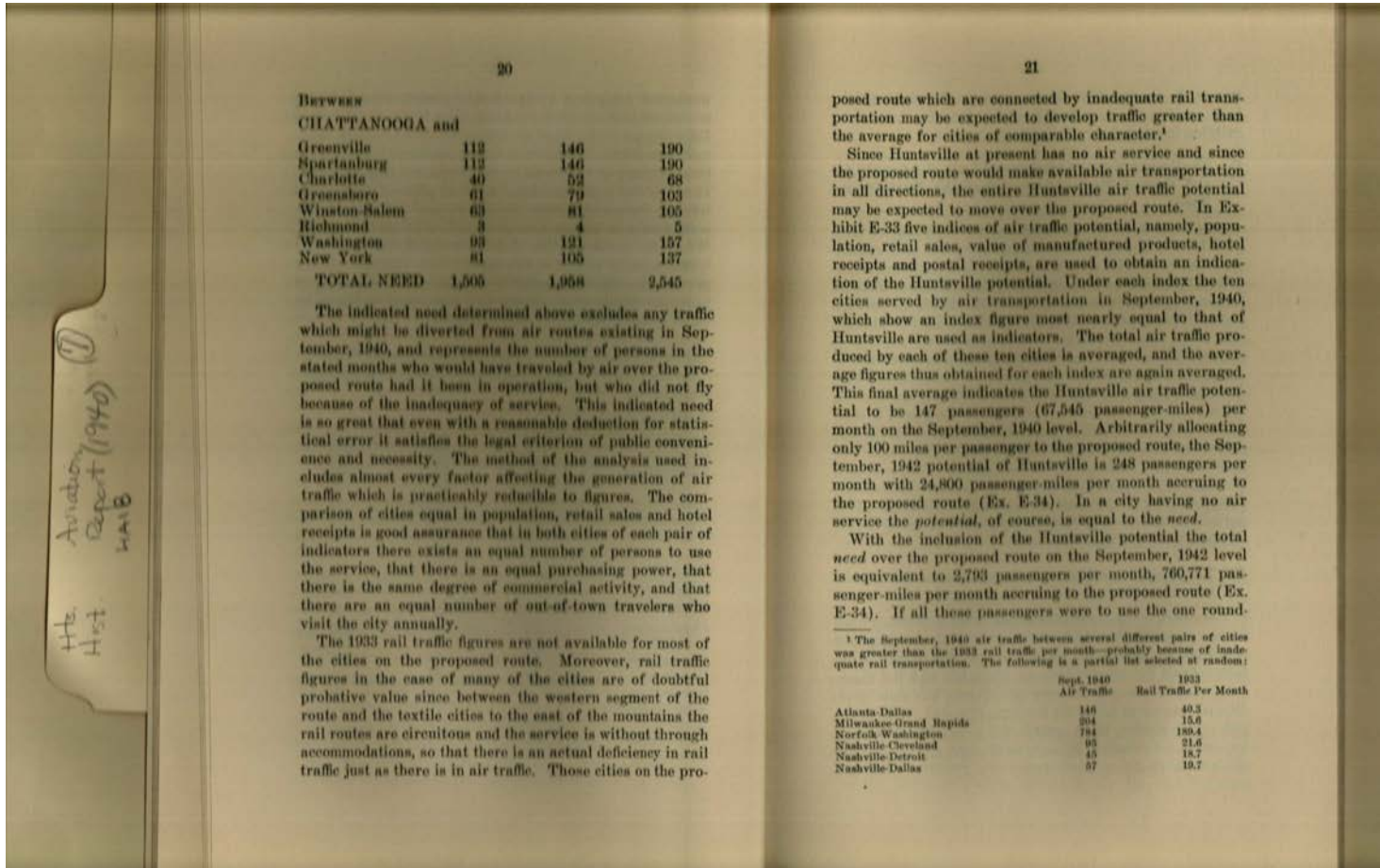


Names:

Statistical Analysis

Types:

book



BETWEEN

CHATTANOOGA and

Greenville	112	146	190
Spartanburg	112	146	190
Charlotte	40	53	68
Greensboro	61	79	103
Winston-Salem	63	81	105
Richmond	8	4	5
Washington	93	121	157
New York	81	105	137
TOTAL NEED	1,565	1,958	2,545

The indicated need determined above excludes any traffic which might be diverted from air routes existing in September, 1940, and represents the number of persons in the stated months who would have traveled by air over the proposed route had it been in operation, but who did not fly because of the inadequacy of service. This indicated need is so great that even with a reasonable deduction for statistical error it satisfies the legal criterion of public convenience and necessity. The method of the analysis used includes almost every factor affecting the generation of air traffic which is practically reducible to figures. The comparison of cities equal in population, retail sales and hotel receipts is good assurance that in both cities of each pair of indicators there exists an equal number of persons to use the service, that there is an equal purchasing power, that there is the same degree of commercial activity, and that there are an equal number of out-of-town travelers who visit the city annually.

The 1933 rail traffic figures are not available for most of the cities on the proposed route. Moreover, rail traffic figures in the case of many of the cities are of doubtful probative value since between the western segment of the route and the textile cities to the east of the mountains the rail routes are circuitous and the service is without through accommodations, so that there is an actual deficiency in rail traffic just as there is in air traffic. Those cities on the pro-

posed route which are connected by inadequate rail transportation may be expected to develop traffic greater than the average for cities of comparable character.¹

Since Huntsville at present has no air service and since the proposed route would make available air transportation in all directions, the entire Huntsville air traffic potential may be expected to move over the proposed route. In Exhibit E-33 five indices of air traffic potential, namely, population, retail sales, value of manufactured products, hotel receipts and postal receipts, are used to obtain an indication of the Huntsville potential. Under each index the ten cities served by air transportation in September, 1940, which show an index figure most nearly equal to that of Huntsville are used as indicators. The total air traffic produced by each of these ten cities is averaged, and the average figures thus obtained for each index are again averaged. This final average indicates the Huntsville air traffic potential to be 147 passengers (67,545 passenger-miles) per month on the September, 1940 level. Arbitrarily allocating only 100 miles per passenger to the proposed route, the September, 1942 potential of Huntsville is 248 passengers per month with 24,800 passenger-miles per month accruing to the proposed route (Ex. E-34). In a city having no air service the *potential*, of course, is equal to the *need*.

With the inclusion of the Huntsville potential the total *need* over the proposed route on the September, 1942 level is equivalent to 2,793 passengers per month, 760,771 passenger-miles per month accruing to the proposed route (Ex. E-34). If all these passengers were to use the one round-

¹The September, 1940 air traffic between several different pairs of cities was greater than the 1933 rail traffic per month—probably because of inadequate rail transportation. The following is a partial list selected at random:

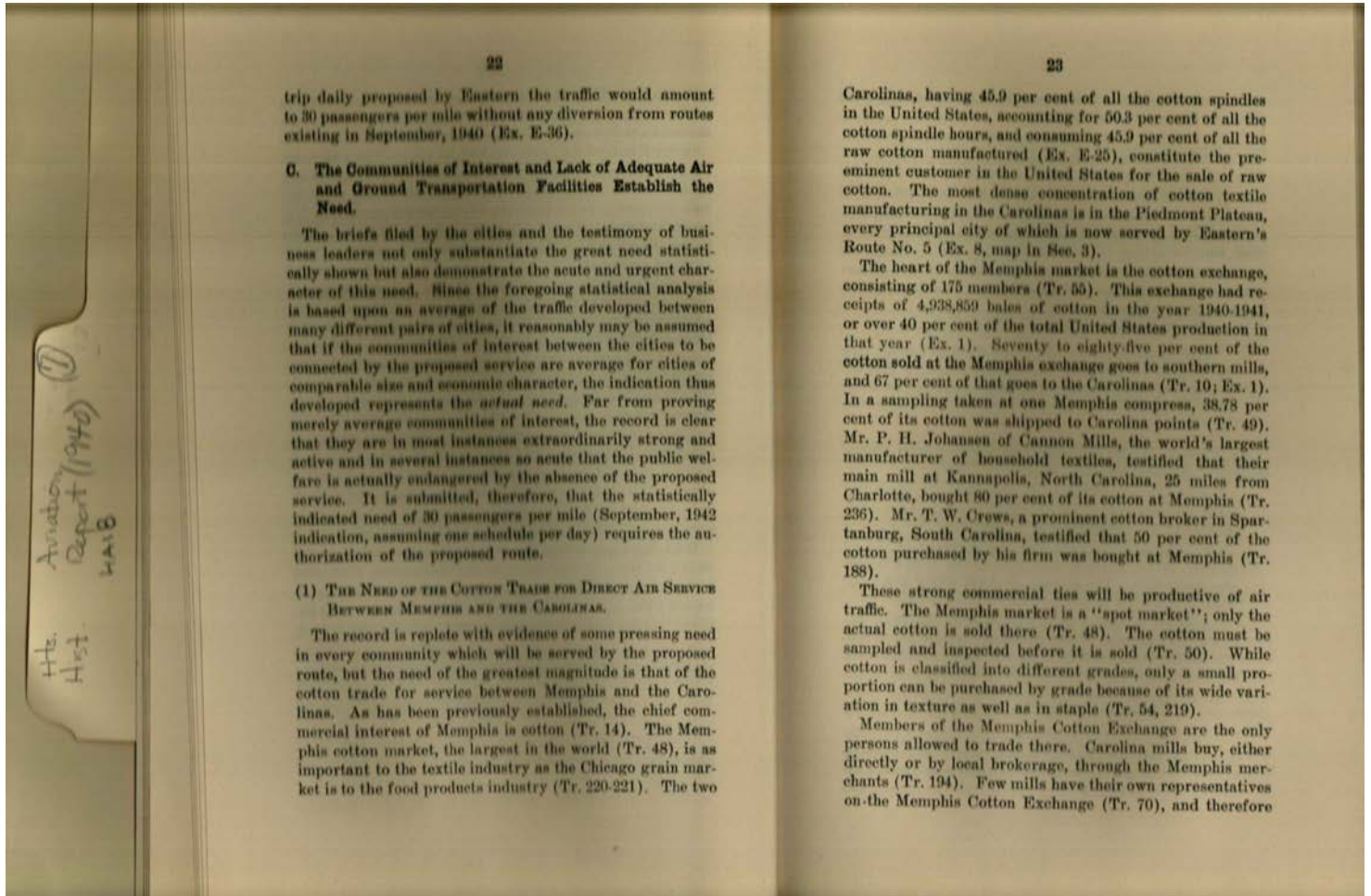
	Sept. 1940 Air Traffic	1933 Rail Traffic Per Month
Atlanta-Dallas	146	40.3
Milwaukee-Grand Rapids	204	15.5
Norfolk-Washington	784	189.4
Nashville-Cleveland	95	21.6
Nashville-Detroit	45	18.7
Nashville-Dallas	57	19.7

Names:

Total Need

Types:

book

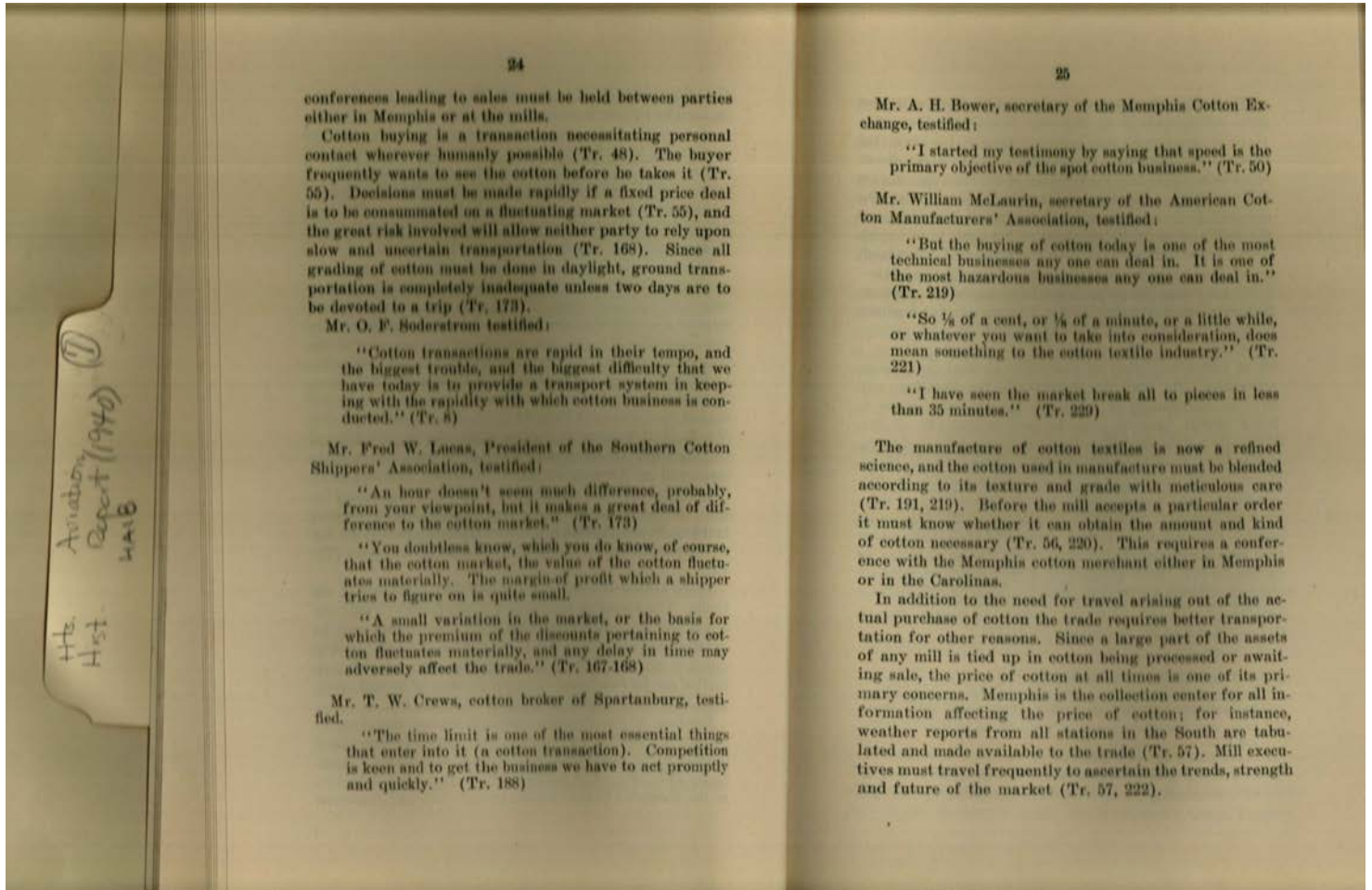


Names:

Communities of
Interest

Types:

book



Names:

Bower, A. H.,

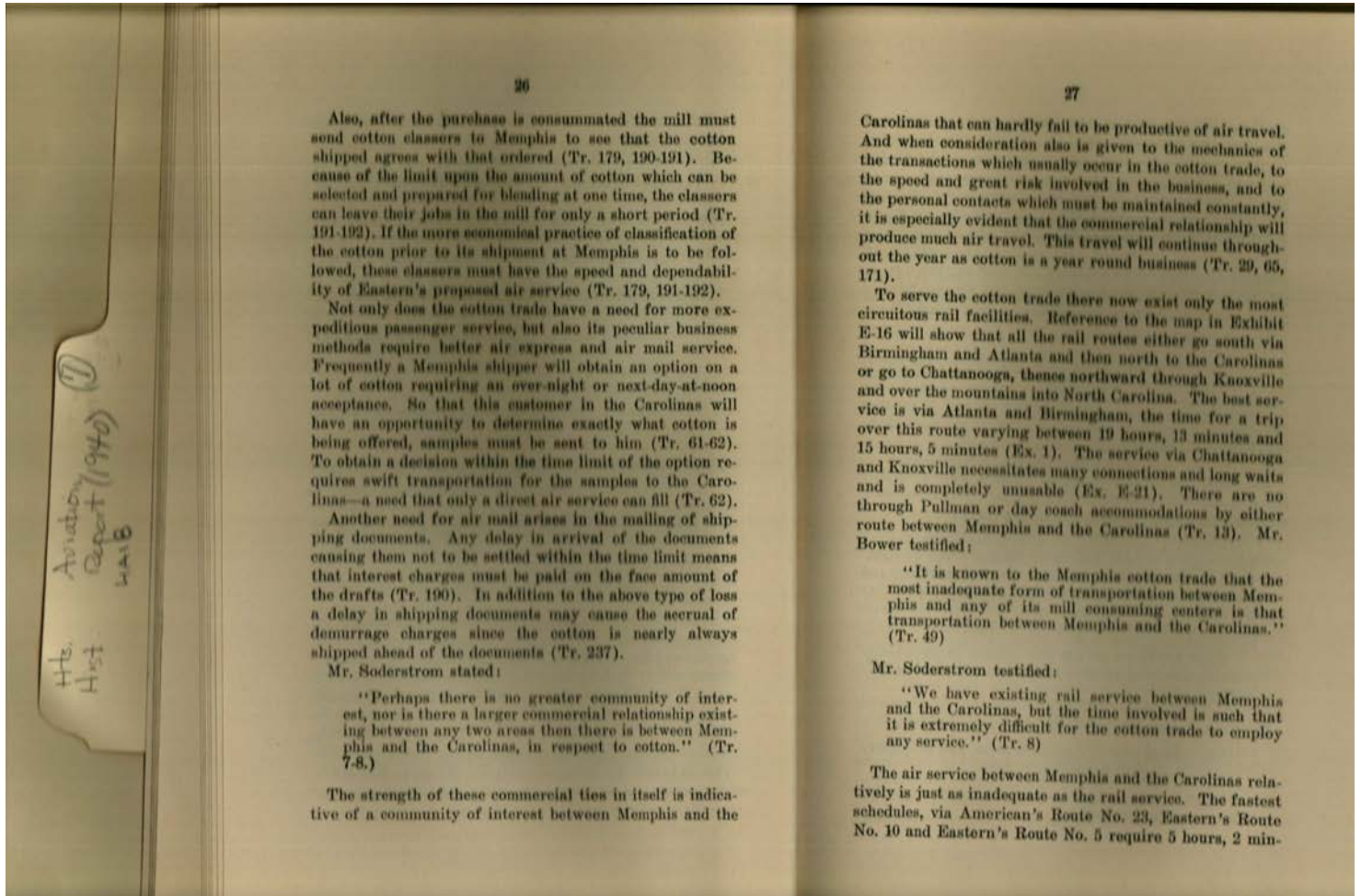
Crews, T. W.

Lucas, Fred. W.

McLaurin, William

Types:

book



Names:

Bower,

Soderstrom,

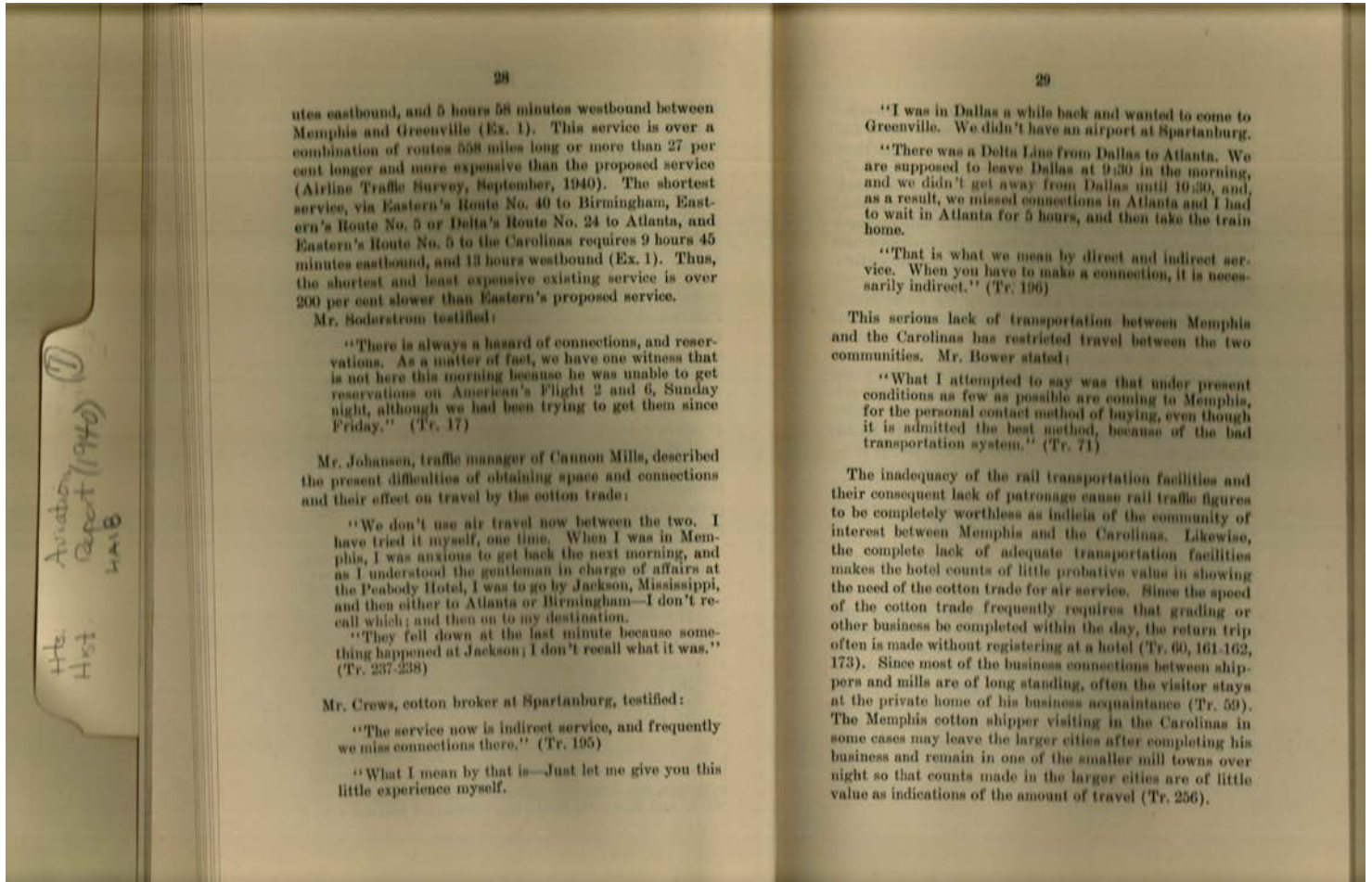
Types:

book

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7

Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 18 r04a18-07-000-0083 [Contents](#) [Index](#) [About](#)



Names:

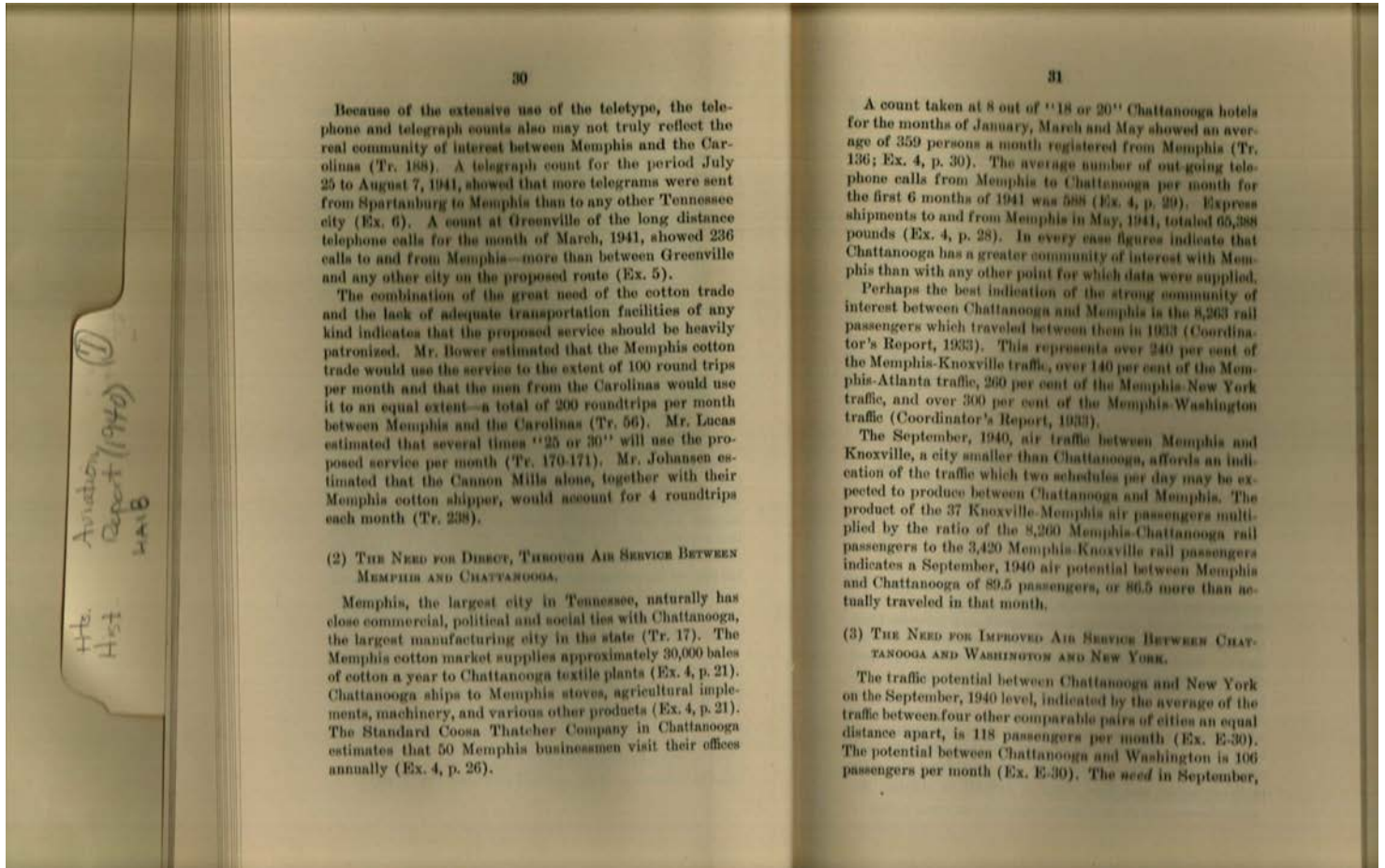
Bower,

Crews,

Johansen,

Types:

book

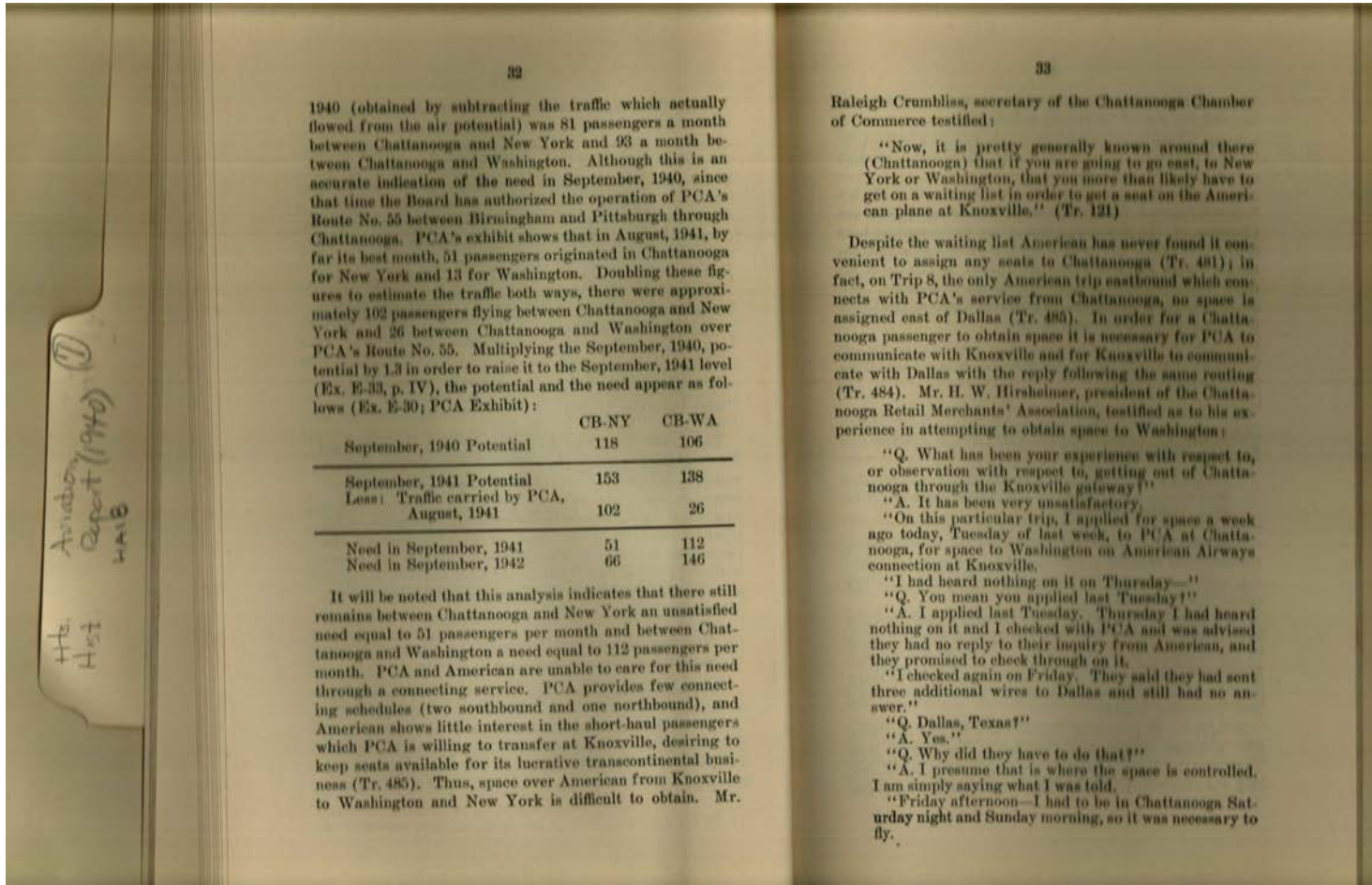


Names:

Memphis to
Chattanooga by air

Types:

book

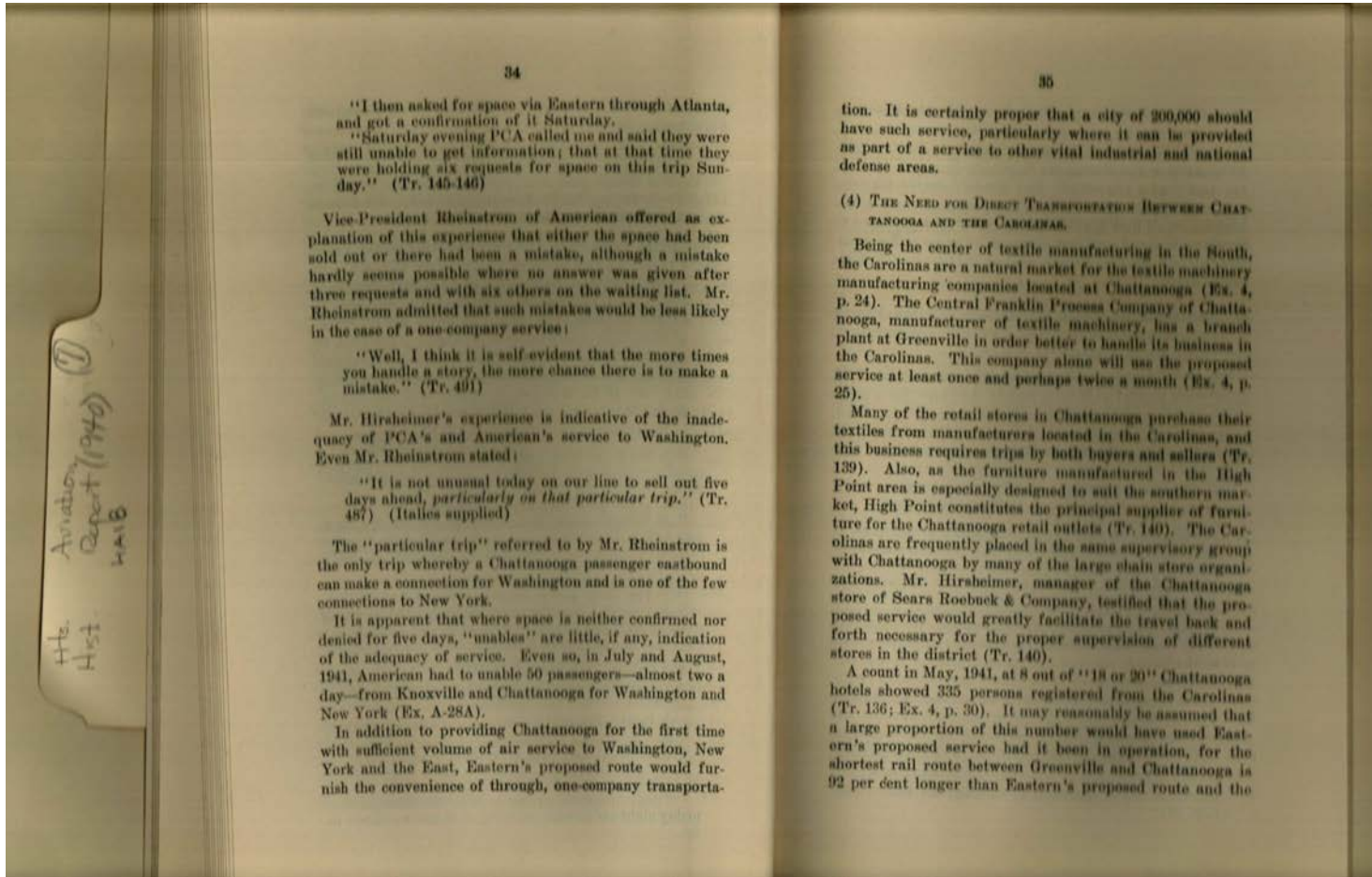


Names:

Crumbliss, Raleigh

Types:

book



Names:

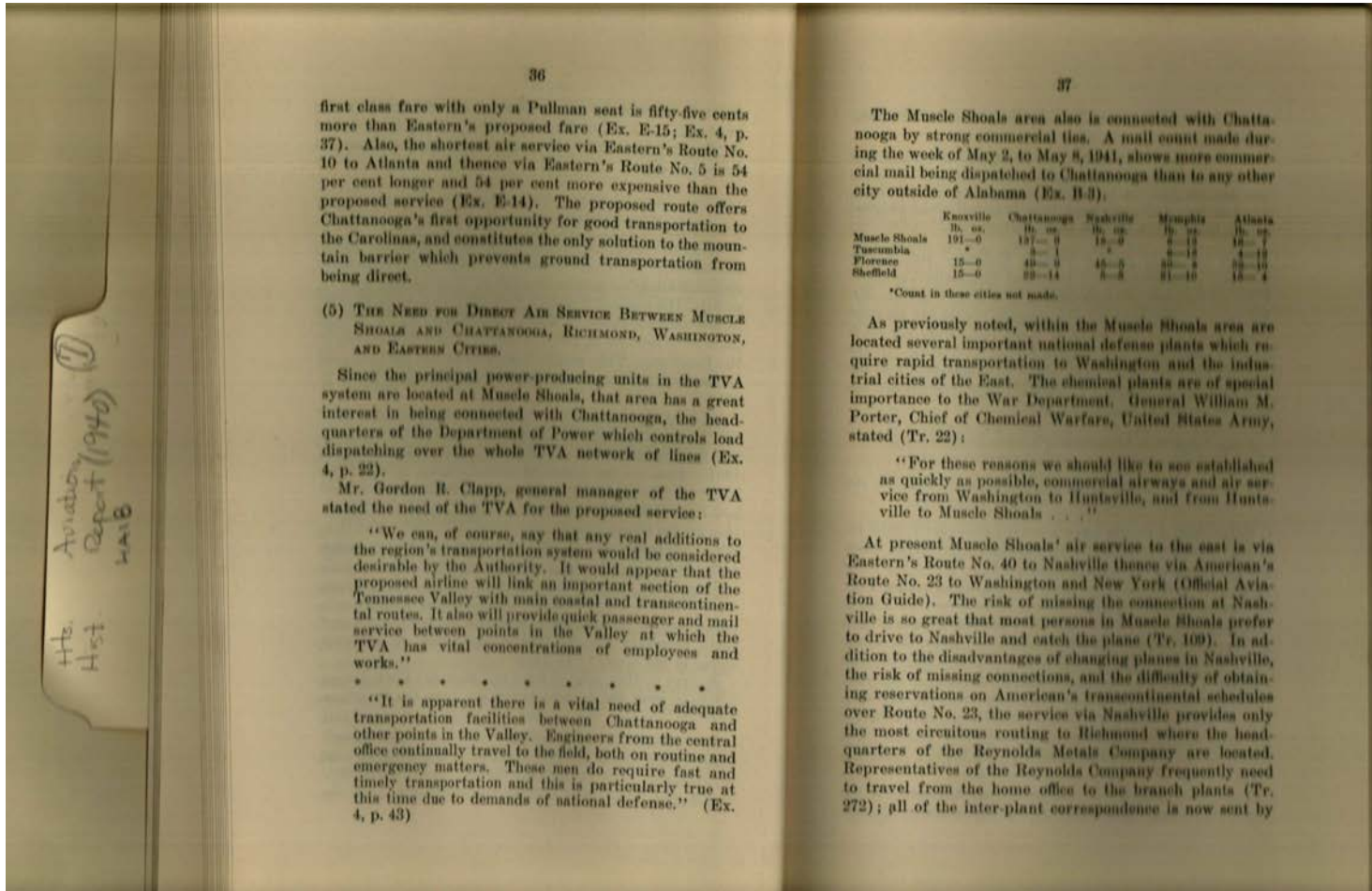
Hirshelmer,

Need for Direct
Transportation

Rheinstrom,

Types:

book



Aviation Report (1940)
 H.H. Hest
 WAIB

36

first class fare with only a Pullman seat is fifty-five cents more than Eastern's proposed fare (Ex. E-15; Ex. 4, p. 37). Also, the shortest air service via Eastern's Route No. 10 to Atlanta and thence via Eastern's Route No. 5 is 54 per cent longer and 54 per cent more expensive than the proposed service (Ex. E-14). The proposed route offers Chattanooga's first opportunity for good transportation to the Carolinas, and constitutes the only solution to the mountain barrier which prevents ground transportation from being direct.

(5) THE NEED FOR DIRECT AIR SERVICE BETWEEN MUSCLE SHOALS AND CHATTANOOGA, RICHMOND, WASHINGTON, AND EASTERN CITIES.

Since the principal power-producing units in the TVA system are located at Muscle Shoals, that area has a great interest in being connected with Chattanooga, the headquarters of the Department of Power which controls load dispatching over the whole TVA network of lines (Ex. 4, p. 22).

Mr. Gordon R. Clapp, general manager of the TVA stated the need of the TVA for the proposed service:

"We can, of course, say that any real additions to the region's transportation system would be considered desirable by the Authority. It would appear that the proposed airline will link an important section of the Tennessee Valley with main coastal and transcontinental routes. It also will provide quick passenger and mail service between points in the Valley at which the TVA has vital concentrations of employees and works."

* * * * *

"It is apparent there is a vital need of adequate transportation facilities between Chattanooga and other points in the Valley. Engineers from the central office continually travel to the field, both on routine and emergency matters. These men do require fast and timely transportation and this is particularly true at this time due to demands of national defense." (Ex. 4, p. 43)

37

The Muscle Shoals area also is connected with Chattanooga by strong commercial ties. A mail count made during the week of May 2, to May 8, 1941, shows more commercial mail being dispatched to Chattanooga than to any other city outside of Alabama (Ex. B-3).

	Knoxville	Chattanooga	Nashville	Memphis	Atlanta
	lb. oz.	lb. oz.	lb. oz.	lb. oz.	lb. oz.
Muscle Shoals	191-0	187-0	15-0	6-12	10-7
Tusculum	*	2-1	*	8-12	4-12
Florence	15-0	40-0	45-5	50-5	50-12
Sheffield	15-0	20-14	5-8	21-10	18-4

*Count in these cities not made.

As previously noted, within the Muscle Shoals area are located several important national defense plants which require rapid transportation to Washington and the industrial cities of the East. The chemical plants are of special importance to the War Department. General William M. Porter, Chief of Chemical Warfare, United States Army, stated (Tr. 22):

"For these reasons we should like to see established as quickly as possible, commercial airways and air service from Washington to Huntsville, and from Huntsville to Muscle Shoals . . ."

At present Muscle Shoals' air service to the east is via Eastern's Route No. 40 to Nashville thence via American's Route No. 23 to Washington and New York (Official Aviation Guide). The risk of missing the connection at Nashville is so great that most persons in Muscle Shoals prefer to drive to Nashville and catch the plane (Tr. 109). In addition to the disadvantages of changing planes in Nashville, the risk of missing connections, and the difficulty of obtaining reservations on American's transcontinental schedules over Route No. 23, the service via Nashville provides only the most circuitous routing to Richmond where the headquarters of the Reynolds Metals Company are located. Representatives of the Reynolds Company frequently need to travel from the home office to the branch plants (Tr. 272); all of the inter-plant correspondence is now sent by

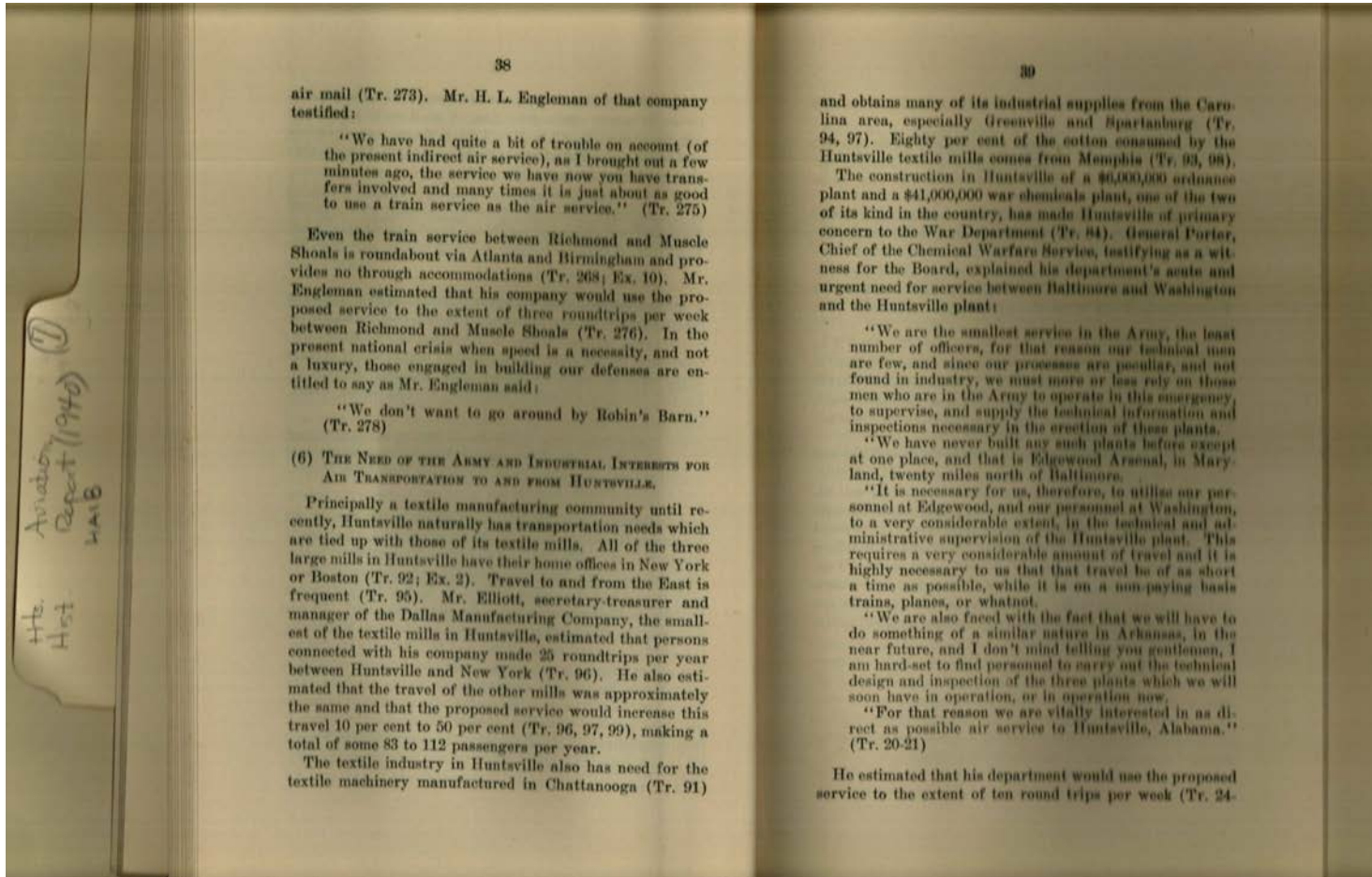
Names:

Clapp, Gordon R.

Porter, William M.,
General

Types:

book



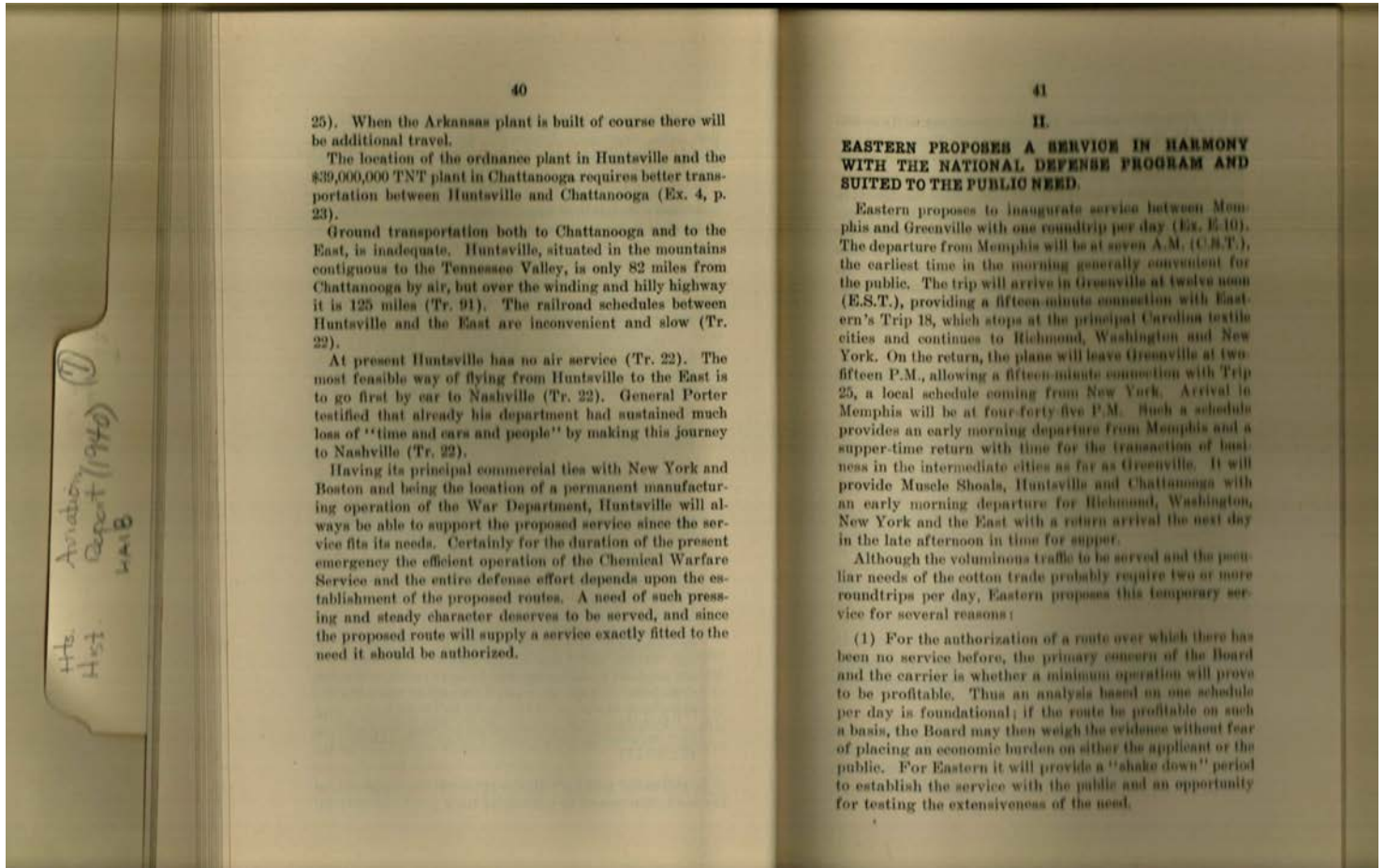
Names:

Army Interests

Engleman, H. L.

Types:

book

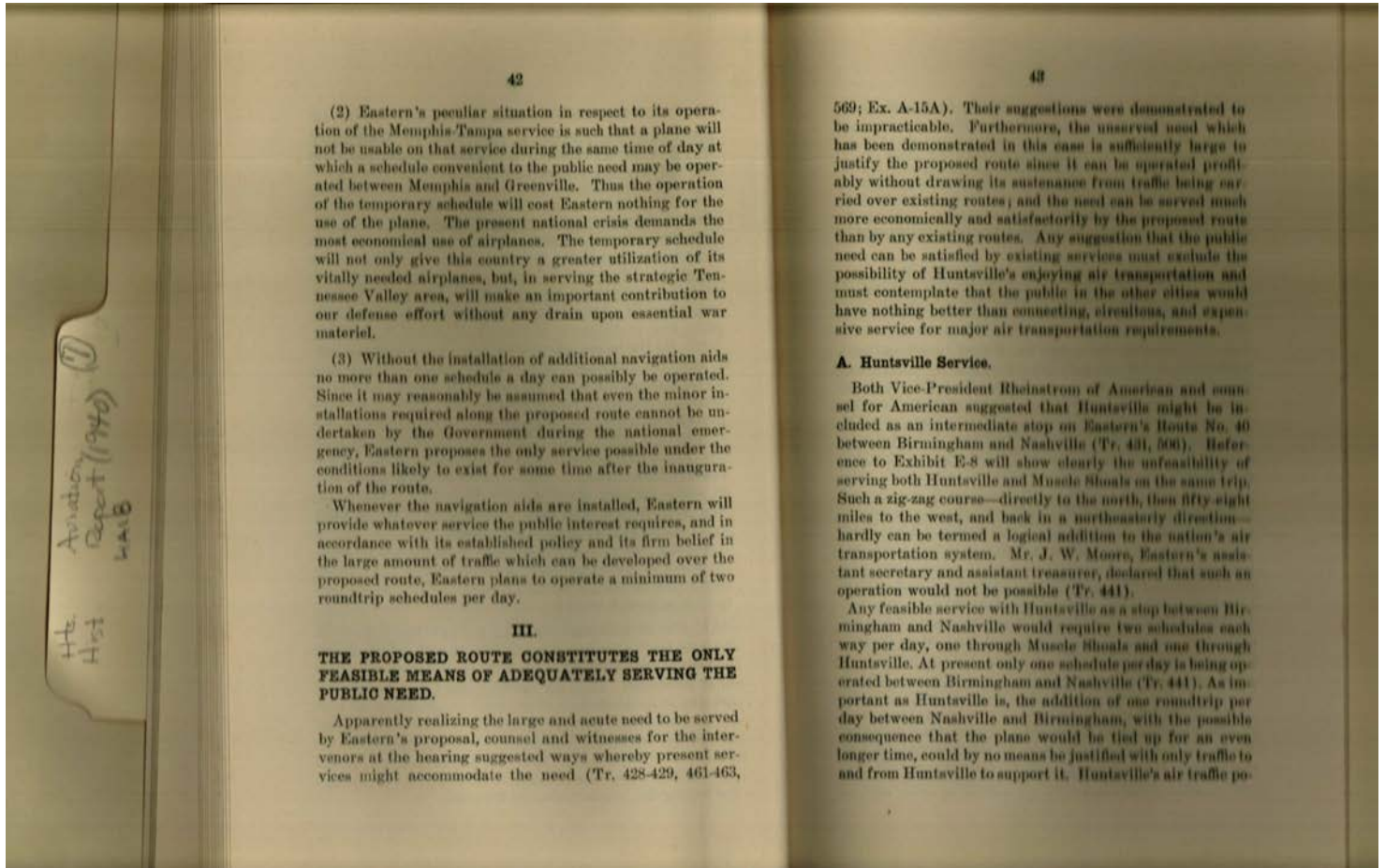


Names:

Eastern Proposed a
Service

Types:

book

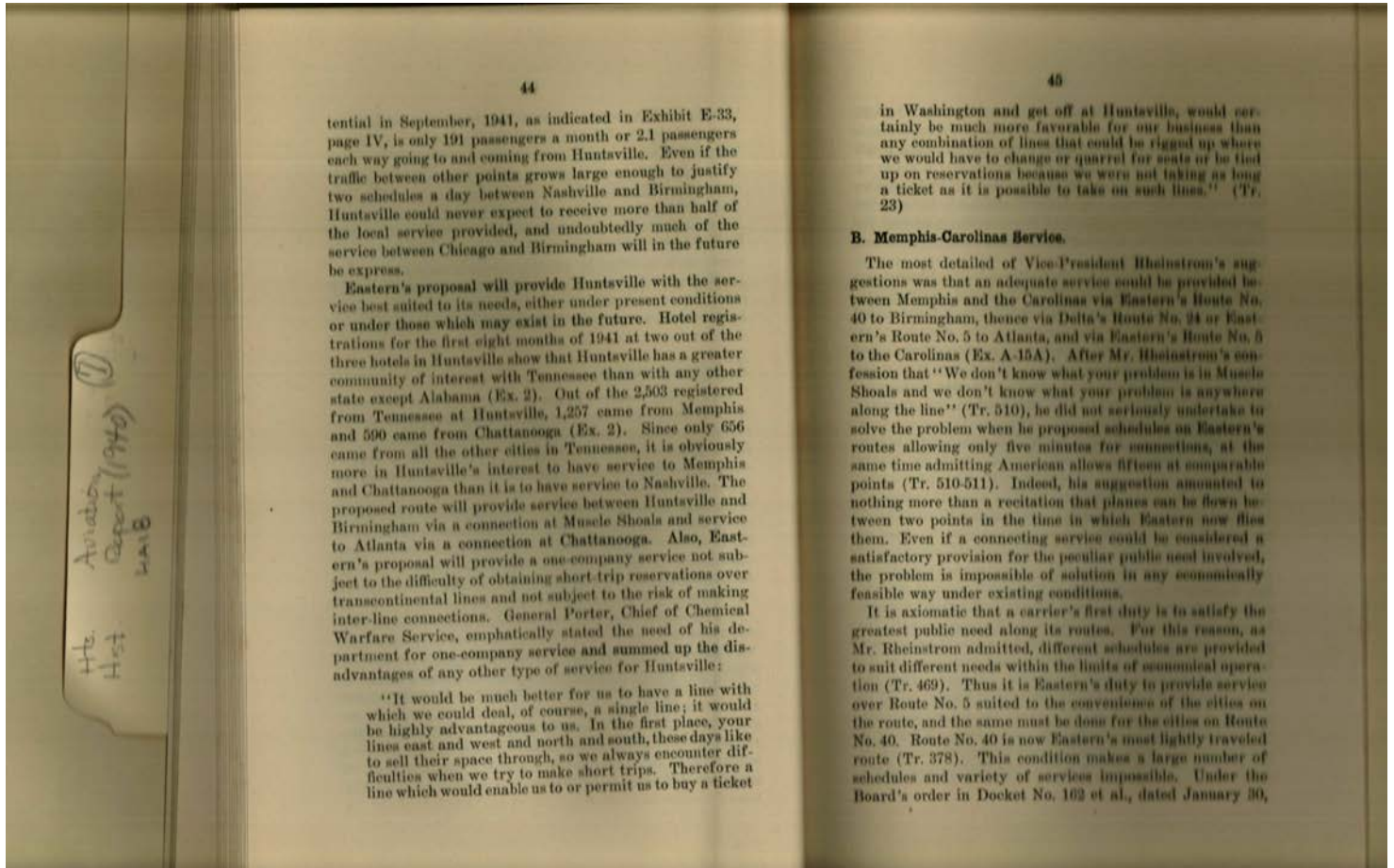


Names:

Public Need for Air
Transportation

Types:

book



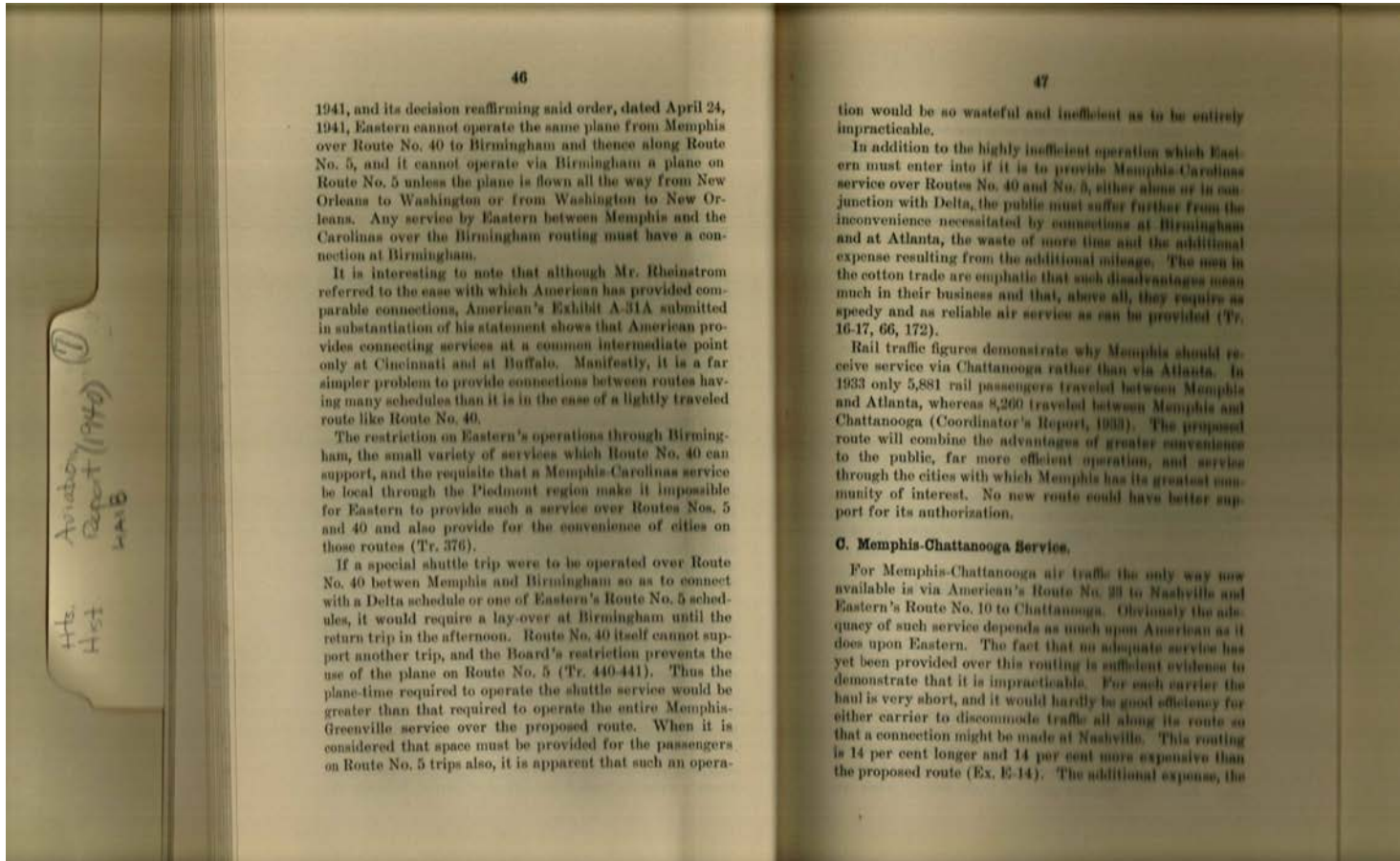
Names:

Memphis-Carolina
Service

Porter, General
Rheinstrom,

Types:

book



Names:

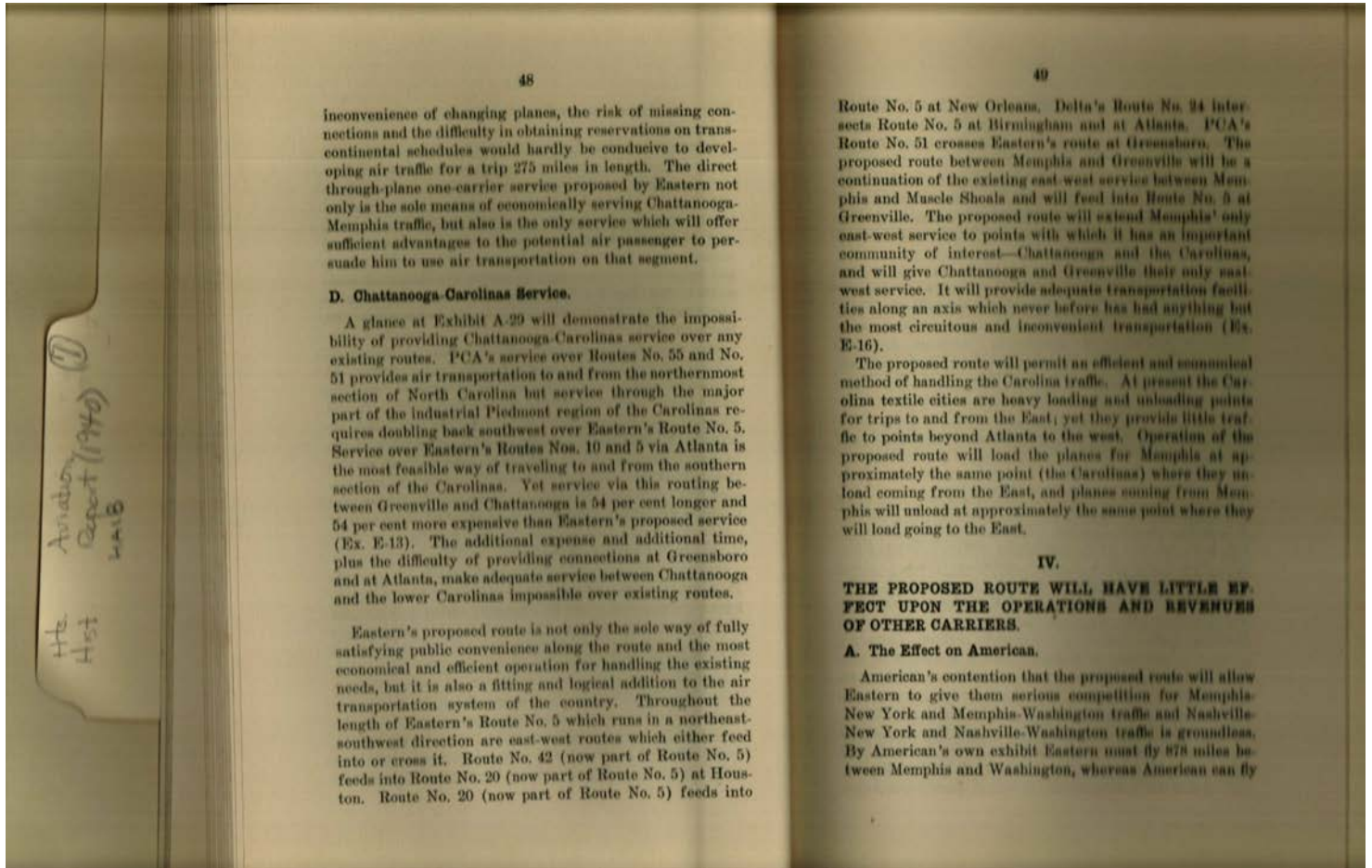
Delta Routes

Eastern Routes

Rheinstrom,

Types:

book

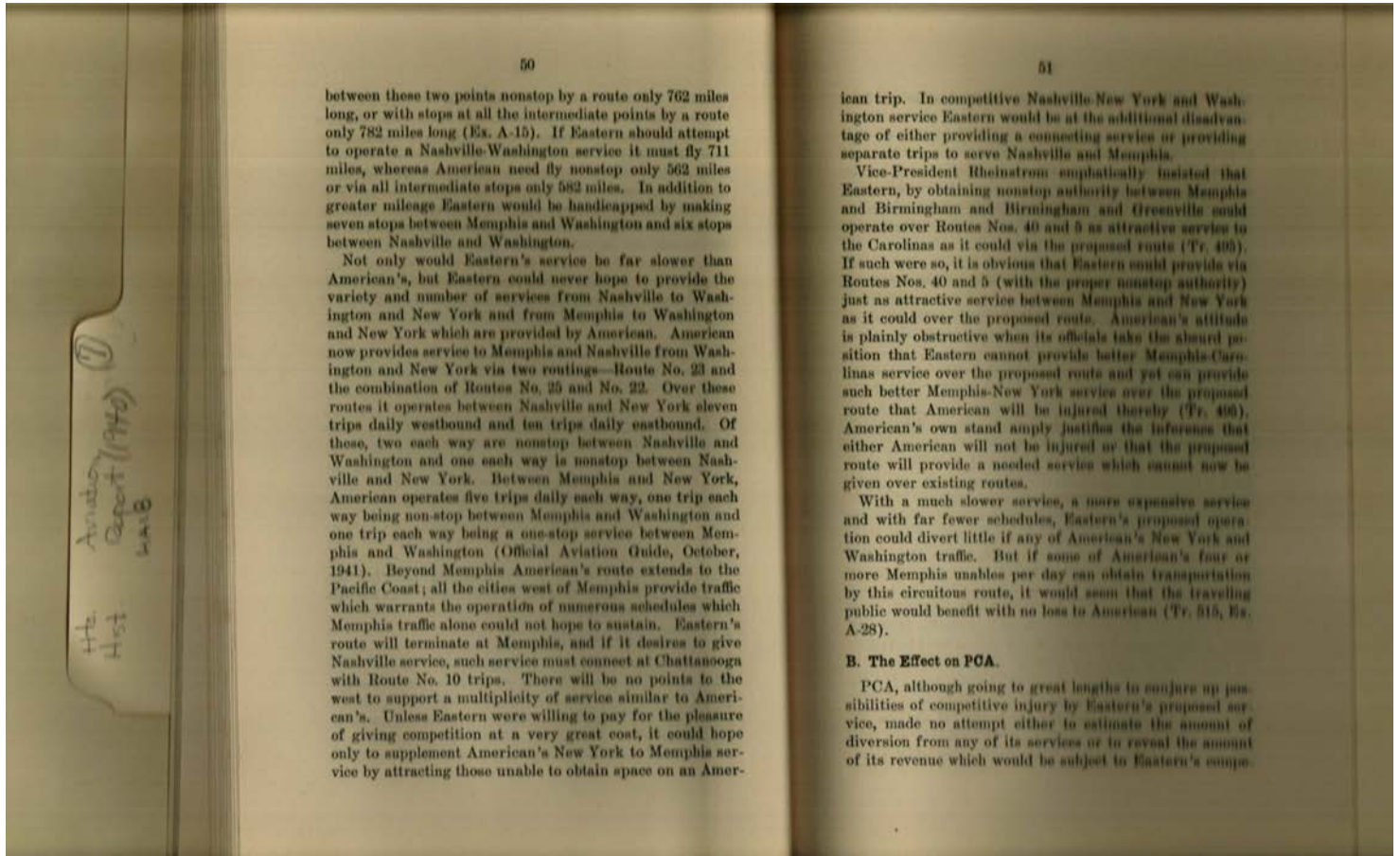


Names:

Effect of Proposed
Route

Types:

book

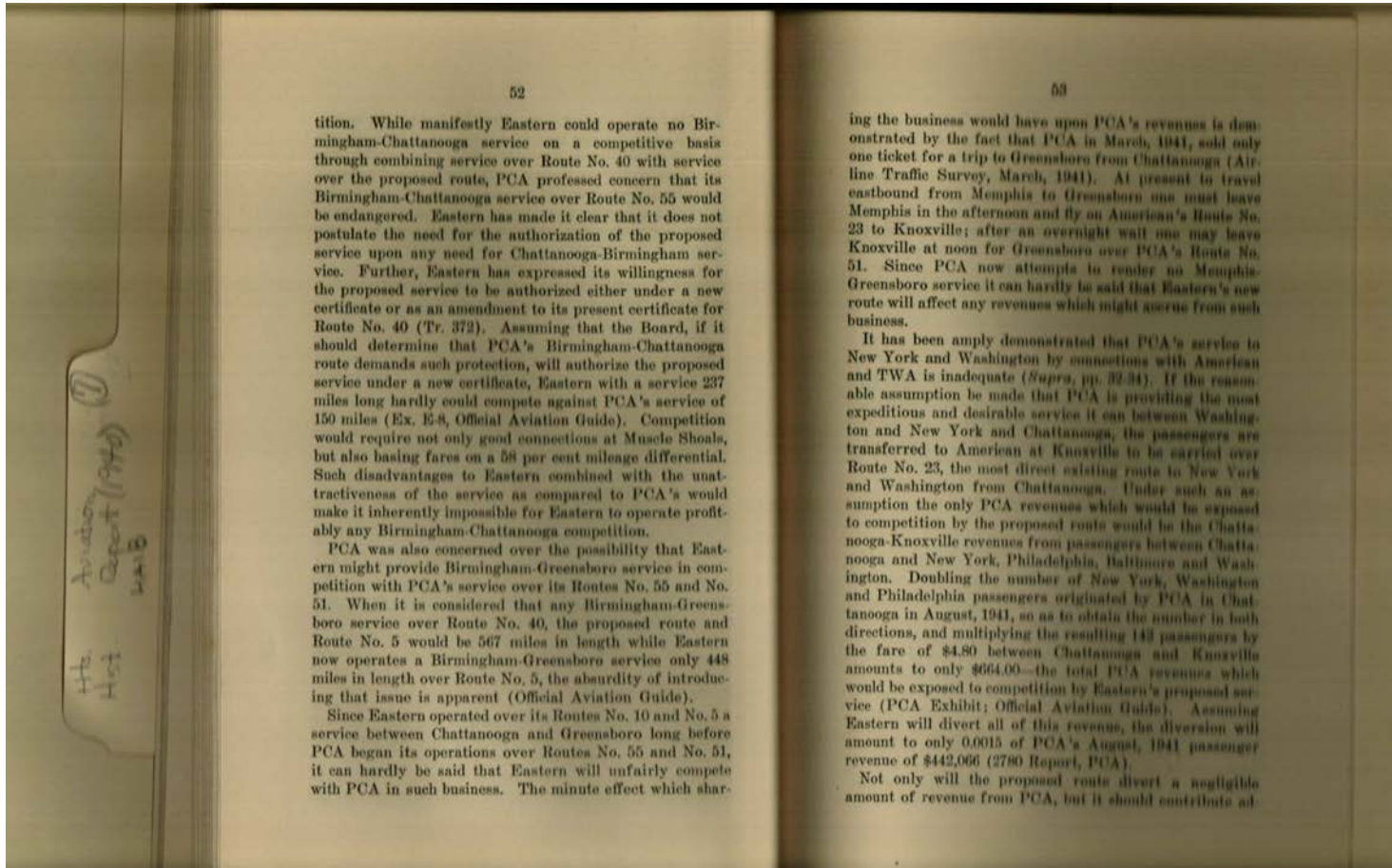


Names:

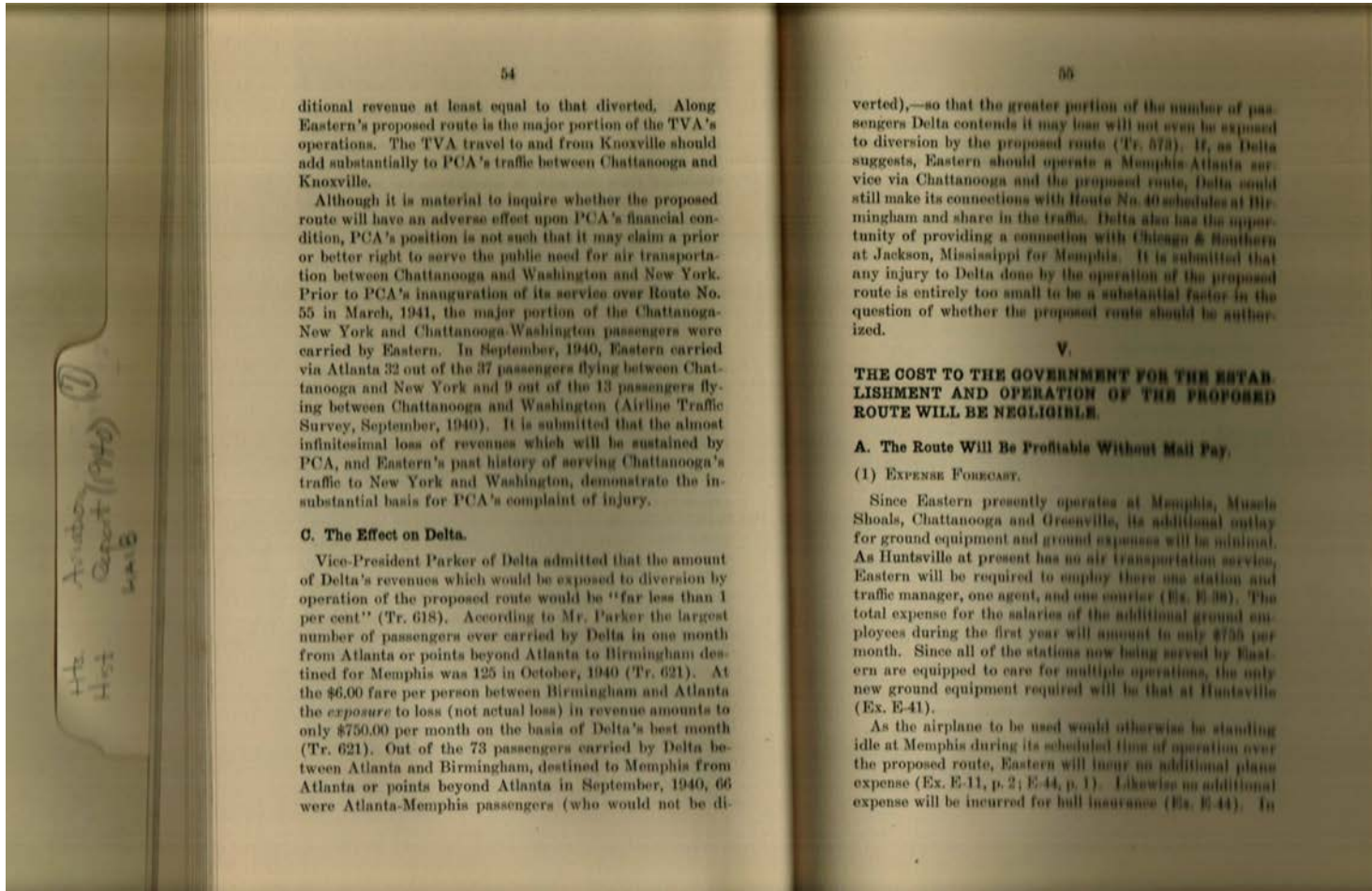
Rheinstrom,

Types:

book



Types:
book



Names:

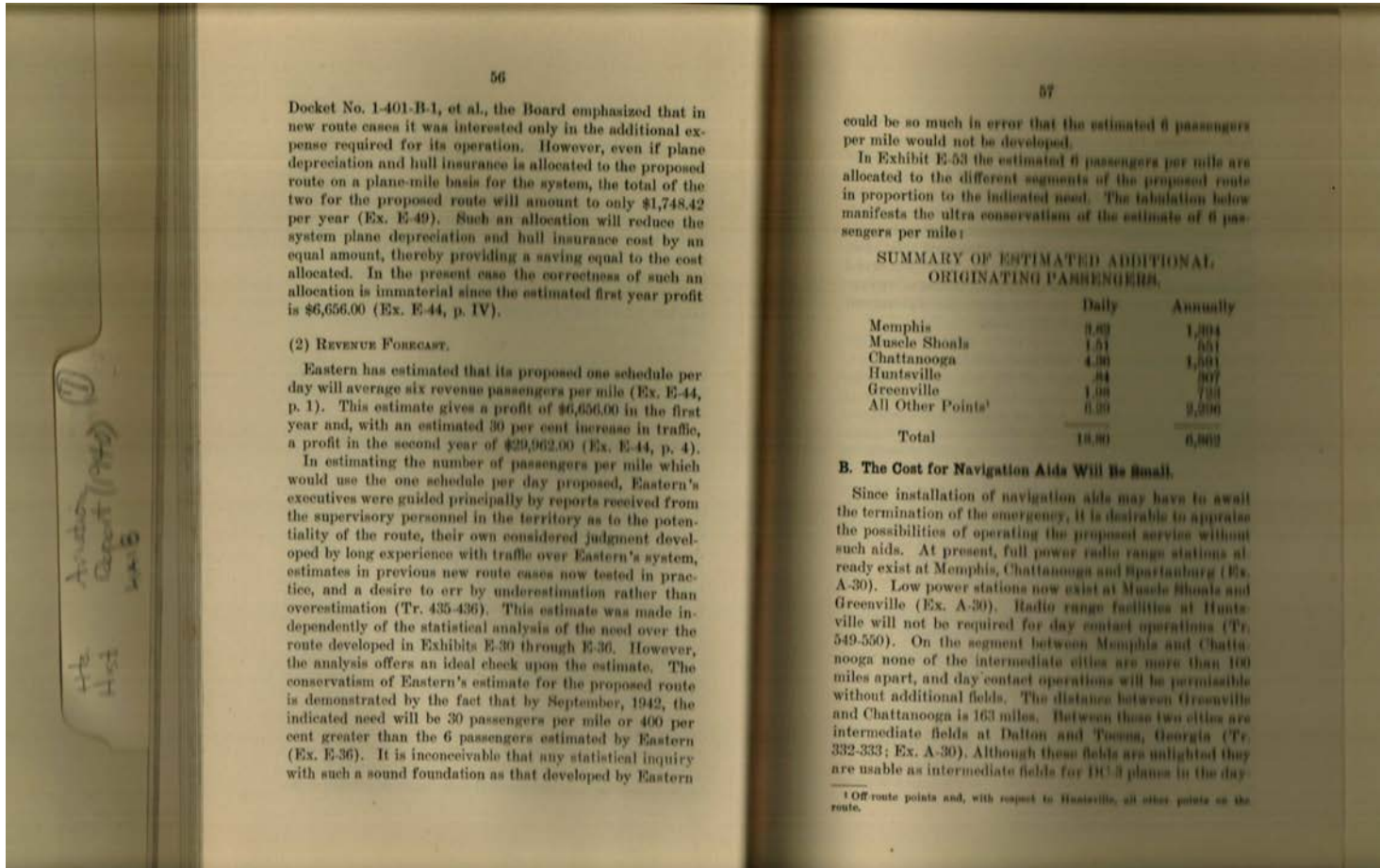
Expense Forecast

Government Costs

Parker,

Types:

book



56

Docket No. 1-401-B-1, et al., the Board emphasized that in new route cases it was interested only in the additional expense required for its operation. However, even if plane depreciation and hull insurance is allocated to the proposed route on a plane-mile basis for the system, the total of the two for the proposed route will amount to only \$1,748.42 per year (Ex. E-49). Such an allocation will reduce the system plane depreciation and hull insurance cost by an equal amount, thereby providing a saving equal to the cost allocated. In the present case the correctness of such an allocation is immaterial since the estimated first year profit is \$6,656.00 (Ex. E-44, p. IV).

(2) REVENUE FORECAST.

Eastern has estimated that its proposed one schedule per day will average six revenue passengers per mile (Ex. E-44, p. 1). This estimate gives a profit of \$6,656.00 in the first year and, with an estimated 30 per cent increase in traffic, a profit in the second year of \$29,962.00 (Ex. E-44, p. 4).

In estimating the number of passengers per mile which would use the one schedule per day proposed, Eastern's executives were guided principally by reports received from the supervisory personnel in the territory as to the potentiality of the route, their own considered judgment developed by long experience with traffic over Eastern's system, estimates in previous new route cases now tested in practice, and a desire to err by underestimation rather than overestimation (Tr. 435-436). This estimate was made independently of the statistical analysis of the need over the route developed in Exhibits E-30 through E-36. However, the analysis offers an ideal check upon the estimate. The conservatism of Eastern's estimate for the proposed route is demonstrated by the fact that by September, 1942, the indicated need will be 30 passengers per mile or 400 per cent greater than the 6 passengers estimated by Eastern (Ex. E-36). It is inconceivable that any statistical inquiry with such a sound foundation as that developed by Eastern

57

could be so much in error that the estimated 6 passengers per mile would not be developed.

In Exhibit E-53 the estimated 6 passengers per mile are allocated to the different segments of the proposed route in proportion to the indicated need. The tabulation below manifests the ultra conservatism of the estimate of 6 passengers per mile:

SUMMARY OF ESTIMATED ADDITIONAL ORIGINATING PASSENGERS.

	Daily	Annually
Memphis	3.89	1,404
Muscle Shoals	1.51	551
Chattanooga	4.30	1,561
Huntsville	.84	307
Greenville	1.08	393
All Other Points ¹	6.30	2,296
Total	18.92	6,962

B. The Cost for Navigation Aids Will Be Small.

Since installation of navigation aids may have to await the termination of the emergency, it is desirable to appraise the possibilities of operating the proposed service without such aids. At present, full power radio range stations already exist at Memphis, Chattanooga and Spartanburg (Ex. A-30). Low power stations now exist at Muscle Shoals and Greenville (Ex. A-30). Radio range facilities at Huntsville will not be required for day contact operations (Tr. 549-550). On the segment between Memphis and Chattanooga none of the intermediate cities are more than 100 miles apart, and day contact operations will be permissible without additional fields. The distance between Greenville and Chattanooga is 163 miles. Between these two cities are intermediate fields at Dalton and Tuscon, Georgia (Tr. 332-333; Ex. A-30). Although these fields are unlighted they are usable as intermediate fields for DC-3 planes in the day.

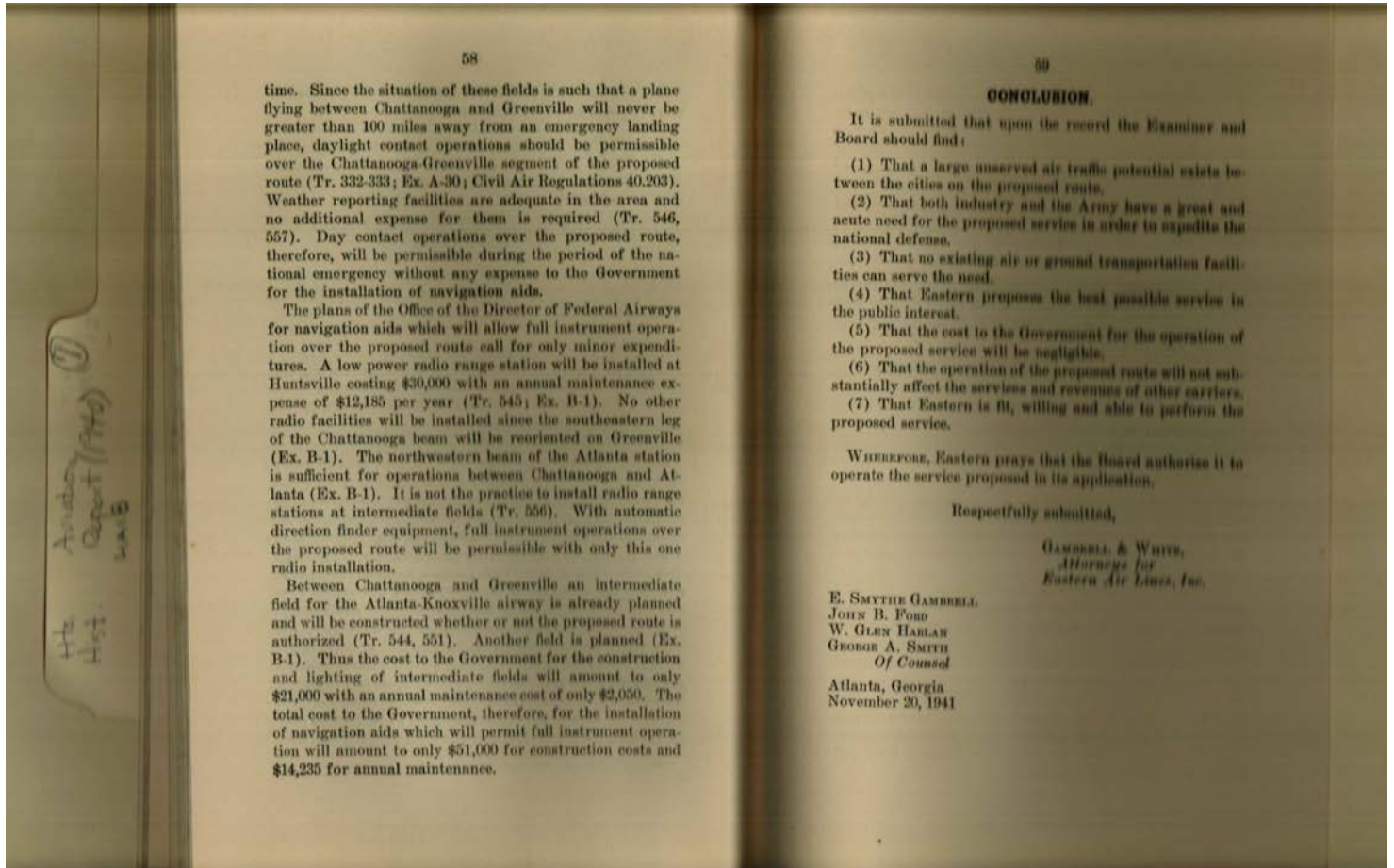
¹ Off-route points and, with respect to Huntsville, all other points on the route.

Names:

Navigation Aids Cost

Types:

book



Names:

Conclusion
Ford, John B.

Gambrell, E. Smythe
Harlan, W. Glen

Smith, George A.

Places:

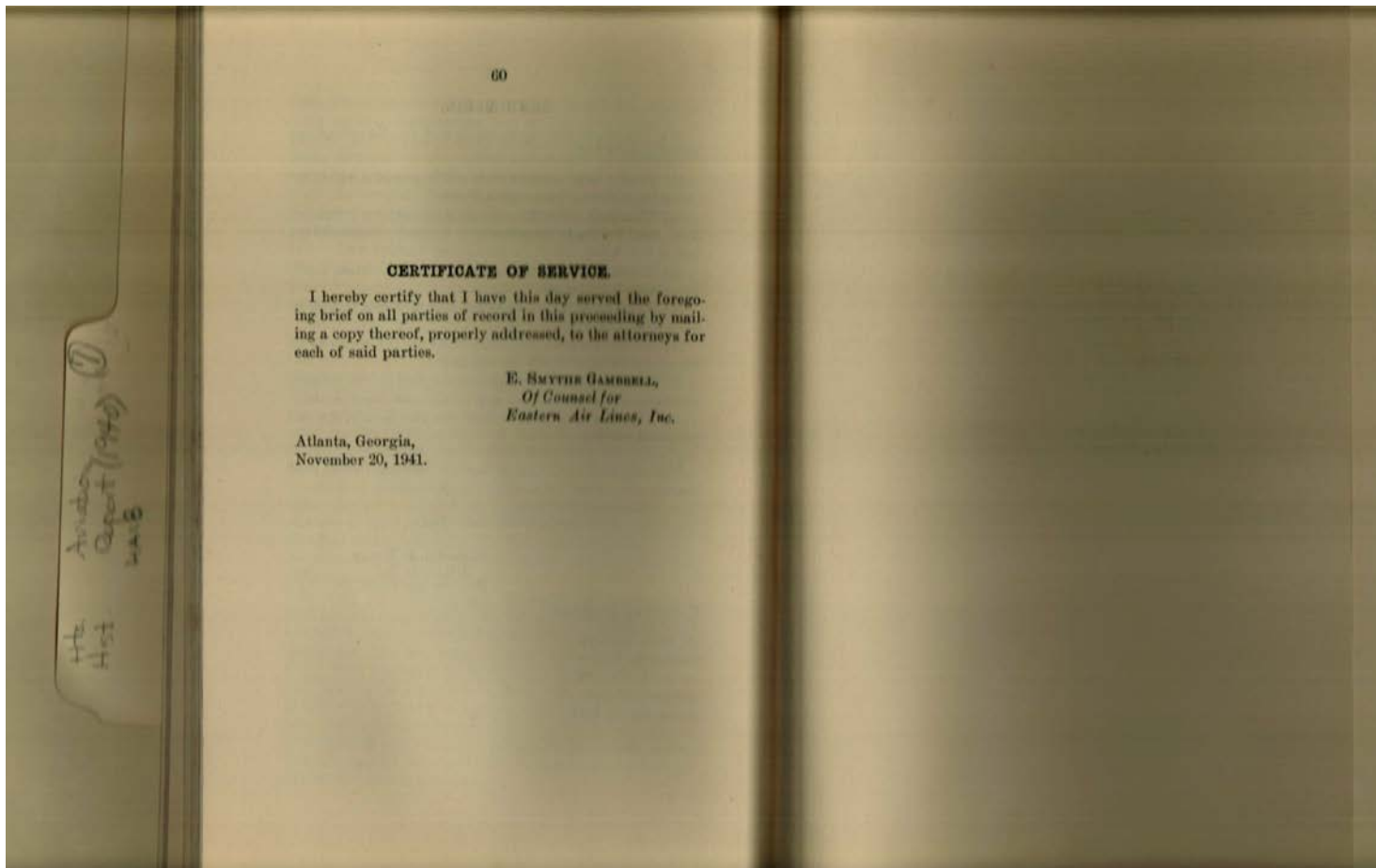
Atlanta, GA

Types:

book

Dates:

Nov 20, 1941



Names:

Gambrell, E. Smythe

Places:

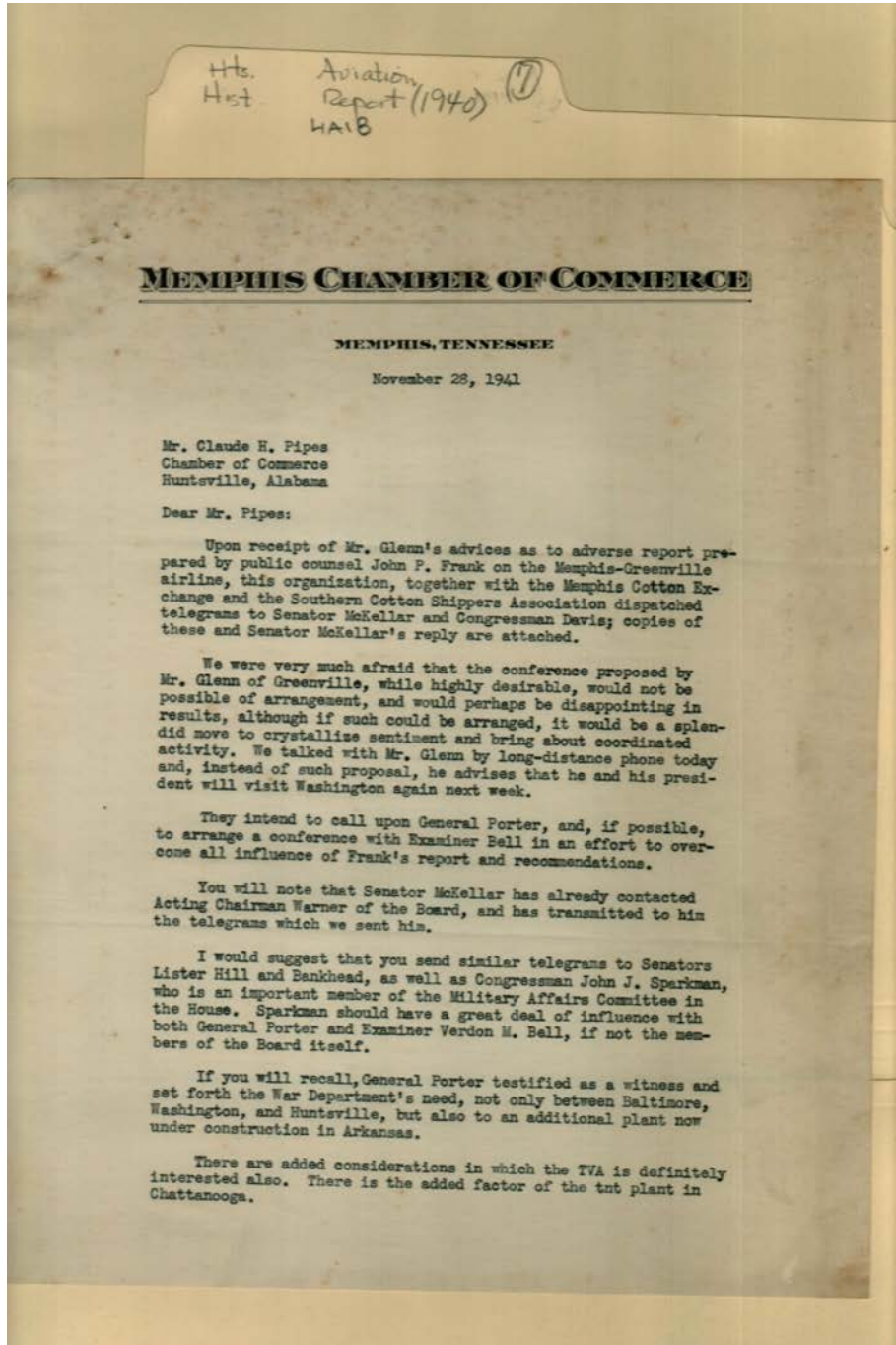
Atlanta, GA

Types:

certificate

Dates:

Nov 20, 1941



Names:

Bankhead, John H.
(Senator)
Bell, Verdon M.
Davis,

Frank, John P.
Glenn,
Hill, Lister
McKellar, Kenneth

Memphis Chamber of
Commerce
Pipes, Claude H.
Porter, General

Sparkman, John J.
(congressman)
Warner,

Places:

Memphis, TN

Types:

correspondence

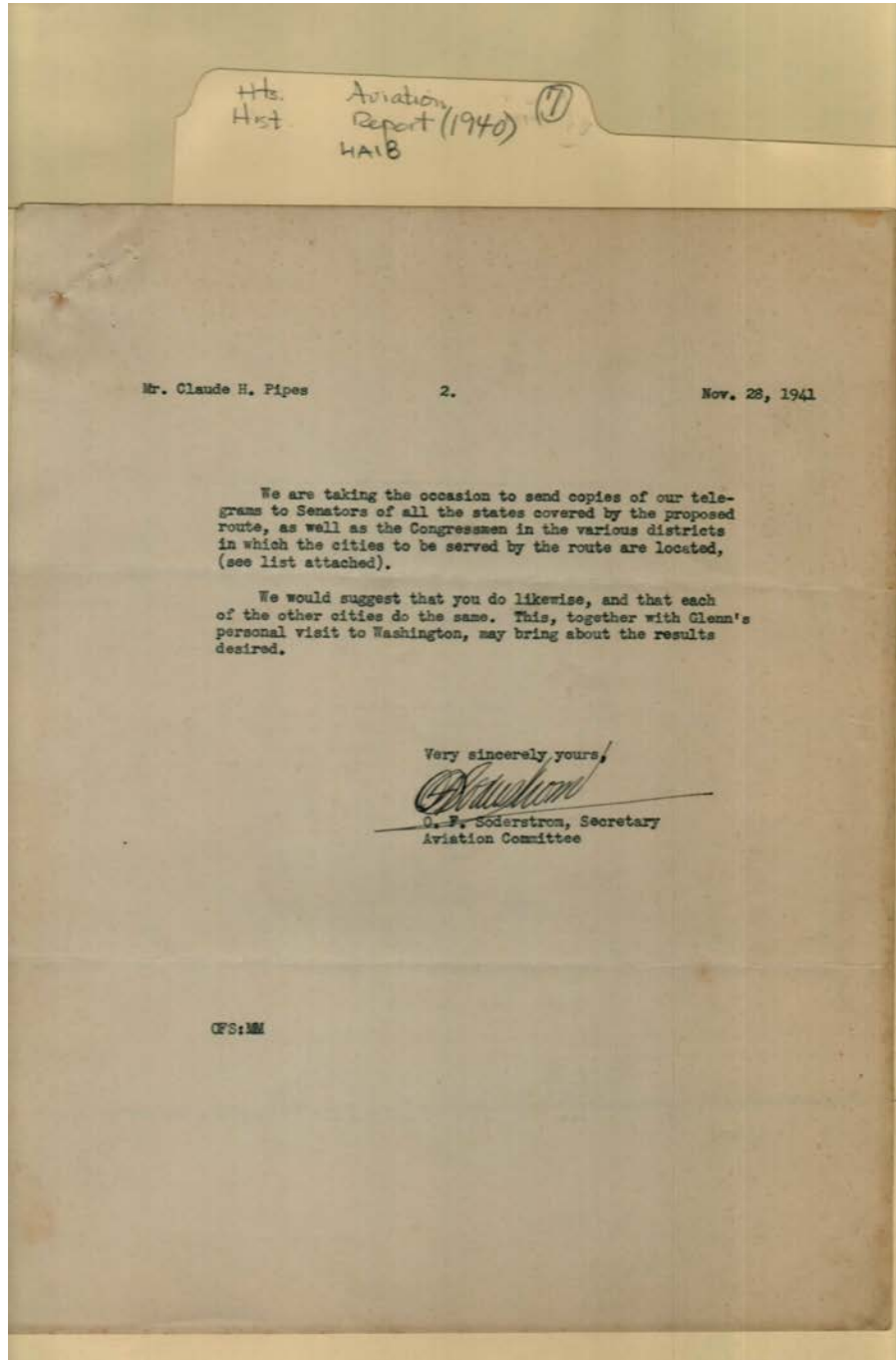
Dates:

Nov 28, 1943

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7

Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 36 r04a18-07-000-0101 [Contents](#) [Index](#) [About](#)



Names:

, Glenn

Pipes, Claude H.

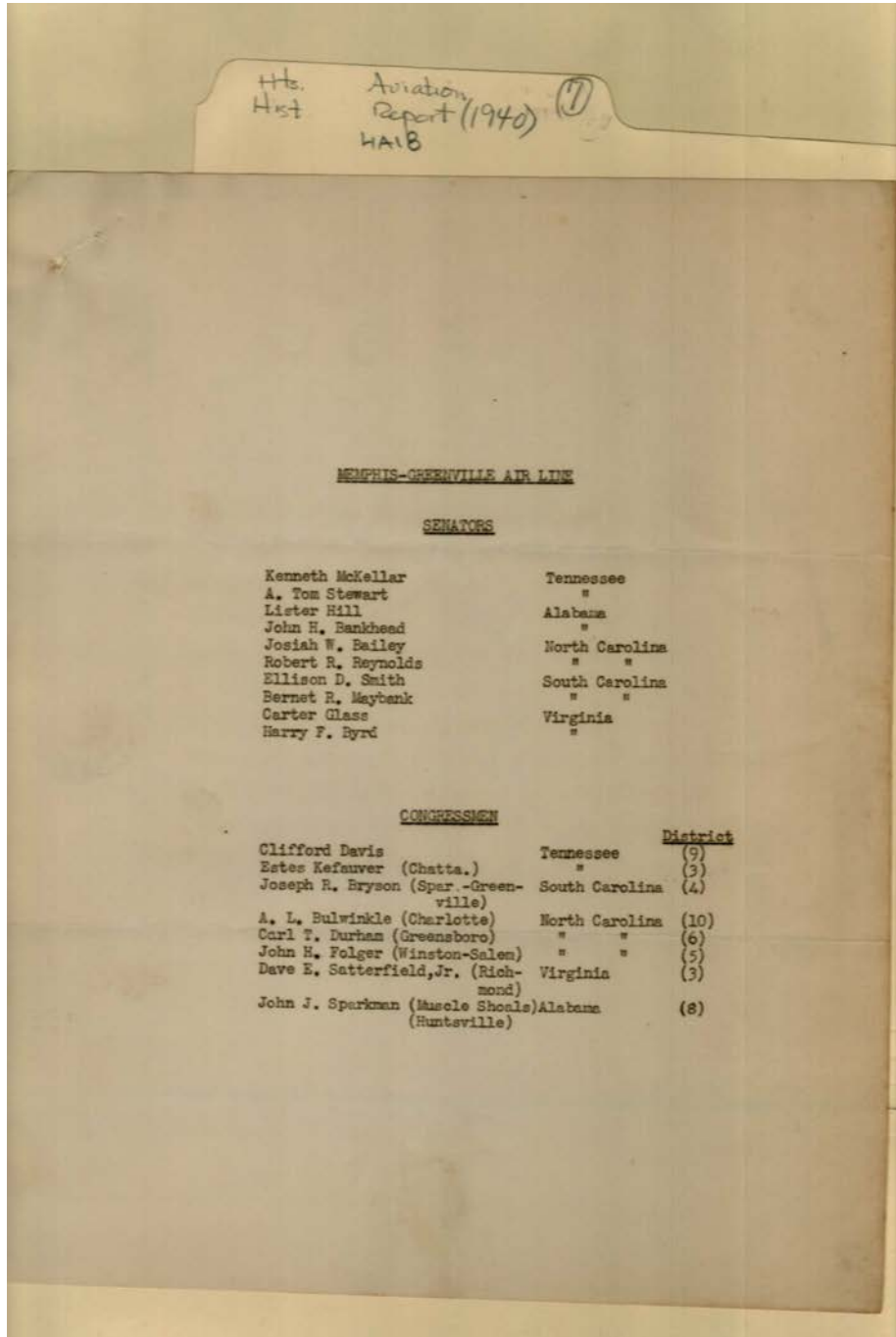
Soderstrom, O. F.

Types:

correspondence

Dates:

Nov 28, 1941



Names:

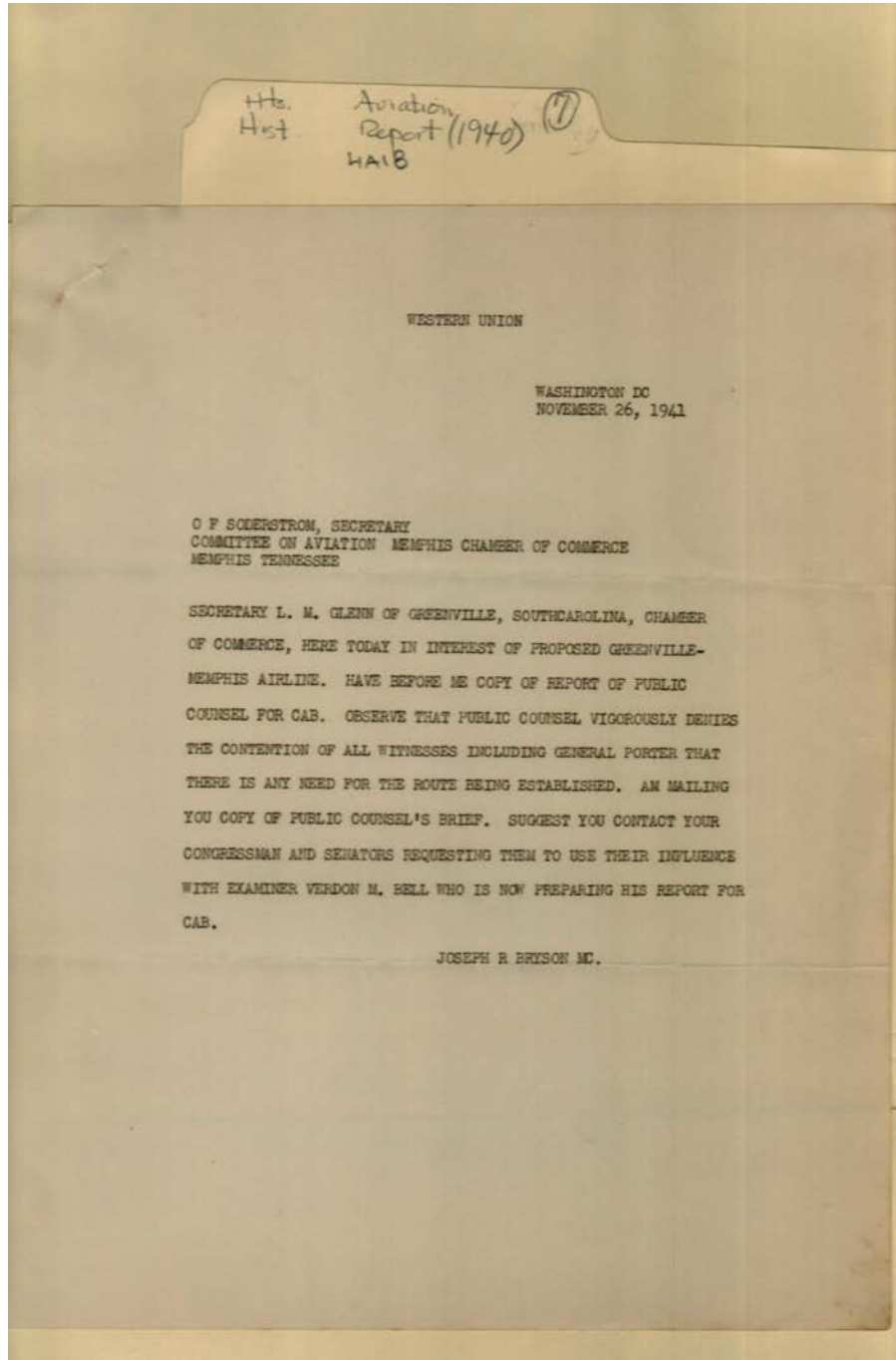
- | | | | |
|------------------------------------|----------------------------------|--|--|
| Bailey, Josiah W.
(Senator) | Davis, Clifford
(congressman) | Kefauver, Estes
(congressman) | Satterfield, Dave E.,
Jr. (congressman) |
| Bankhead, John H.
(Senator) | Durham, Carl T.
(congressman) | Maybank, Bernet R.
(Senator) | Smith, Ellison D.
(Senator) |
| Bryson, Joseph R.
(congressman) | Folger, John H.
(congressman) | McKellar, Kenneth
Memphis-Greenville
Airline | Sparkman, John J.
(congressman) |
| Bulwinkle, A. L.
(congressman) | Glass, Carter
(Senator) | Reynolds, Robert R.
(Senator) | Stewart, A. Tom |
| Byrd, Harry F.
(Senator) | Hill, Lister | | |

Types:
list

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7

Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 38 r04a18-07-000-0103 [Contents](#) [Index](#) [About](#)



Names:

Bell, Verdon M.

Bryson, Joseph R.
(congressman)

Glenn, L. M.
Porter, General

Soderstrom, O. F.

Places:

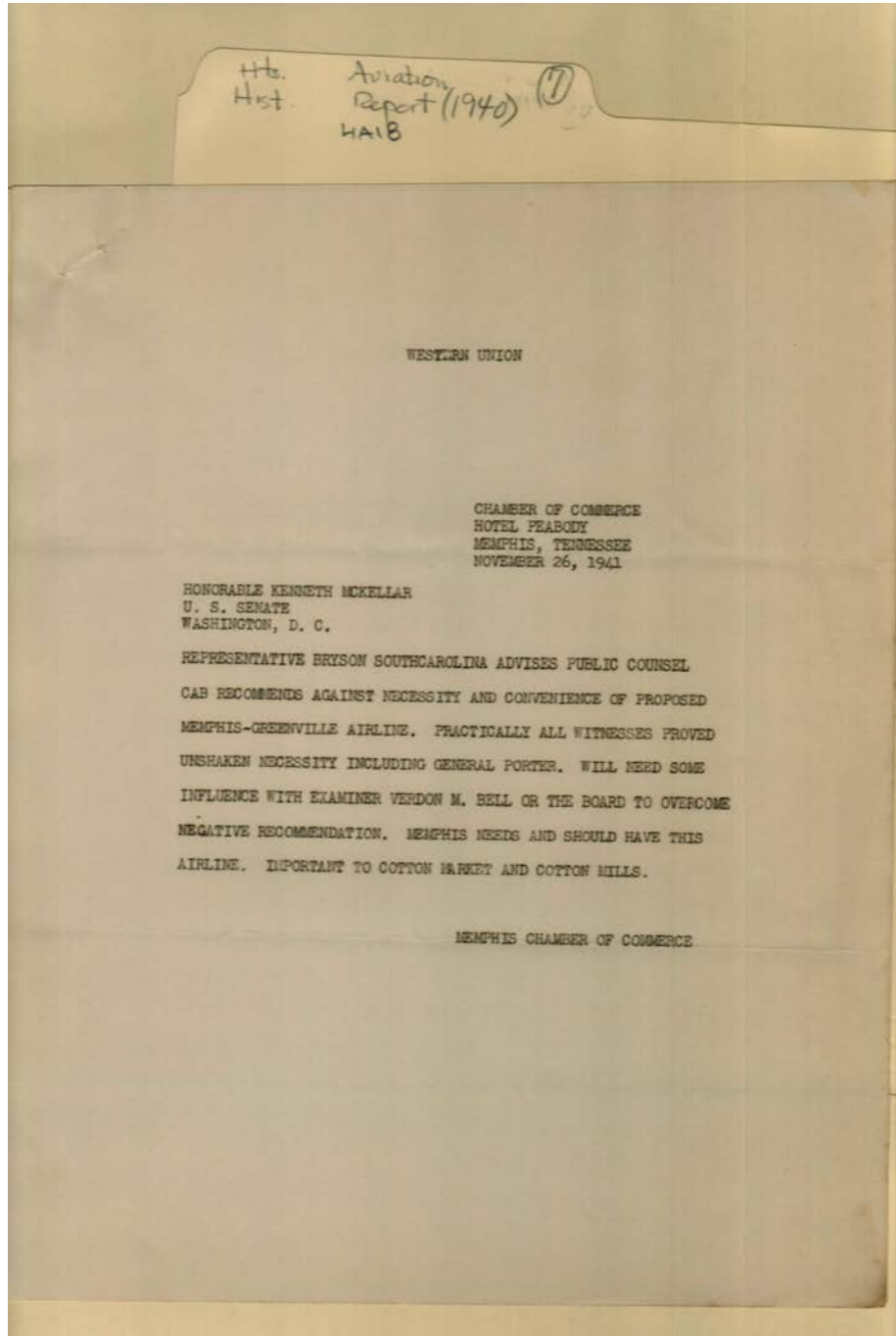
Washington, DC

Types:

telegram

Dates:

Nov 26, 1941



Names:

Bell, Verdon M.
Bryson,

McKellar, Kenneth

Memphis Chamber of
Commerce

Porter, General

Places:

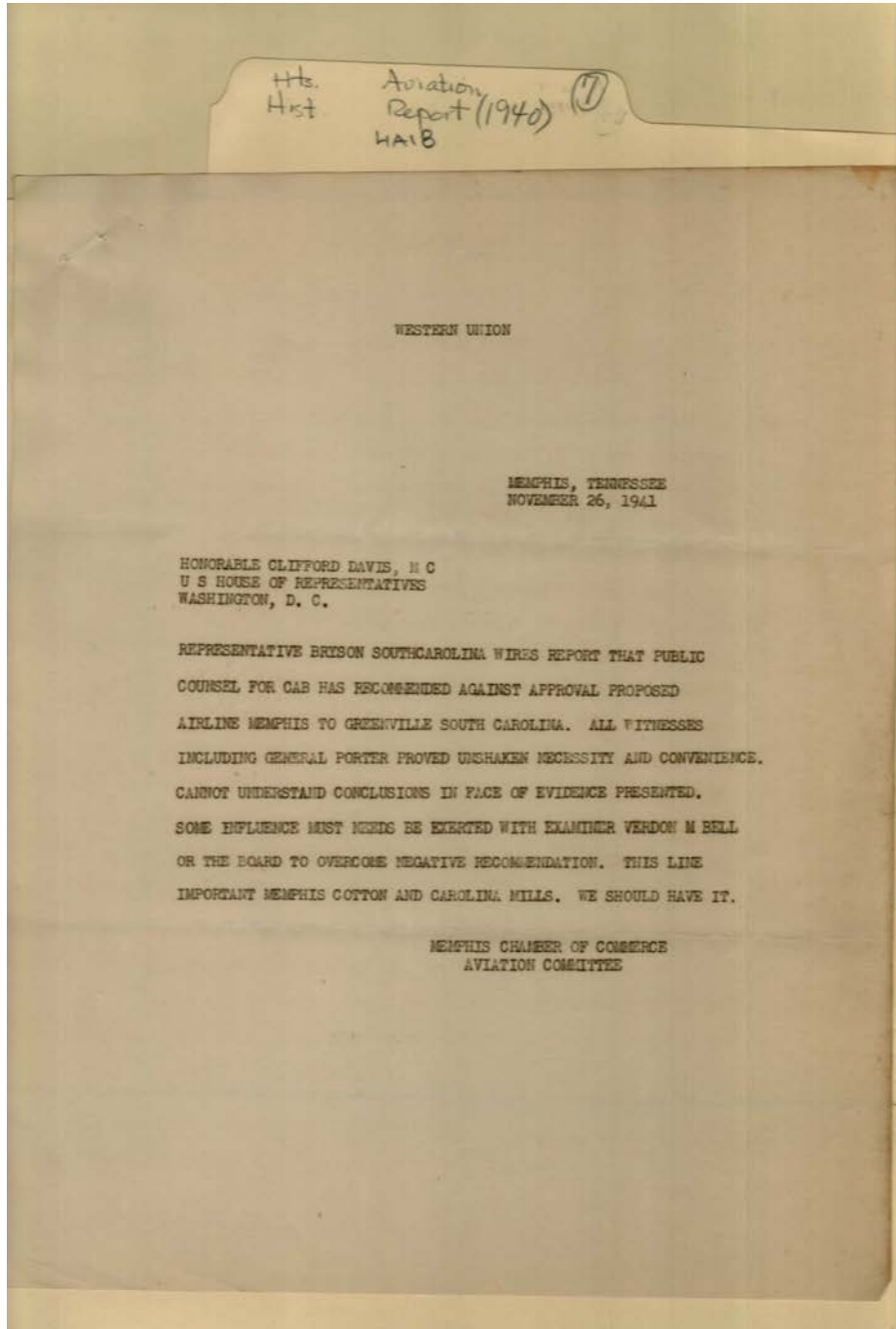
Memphis, TN

Types:

telegram

Dates:

Nov 26, 1941



Names:

Bell, Verdon M.
Bryson,

Davis, Clifford
(congressman)

Memphis Chamber of
Commerce

Porter, General

Places:

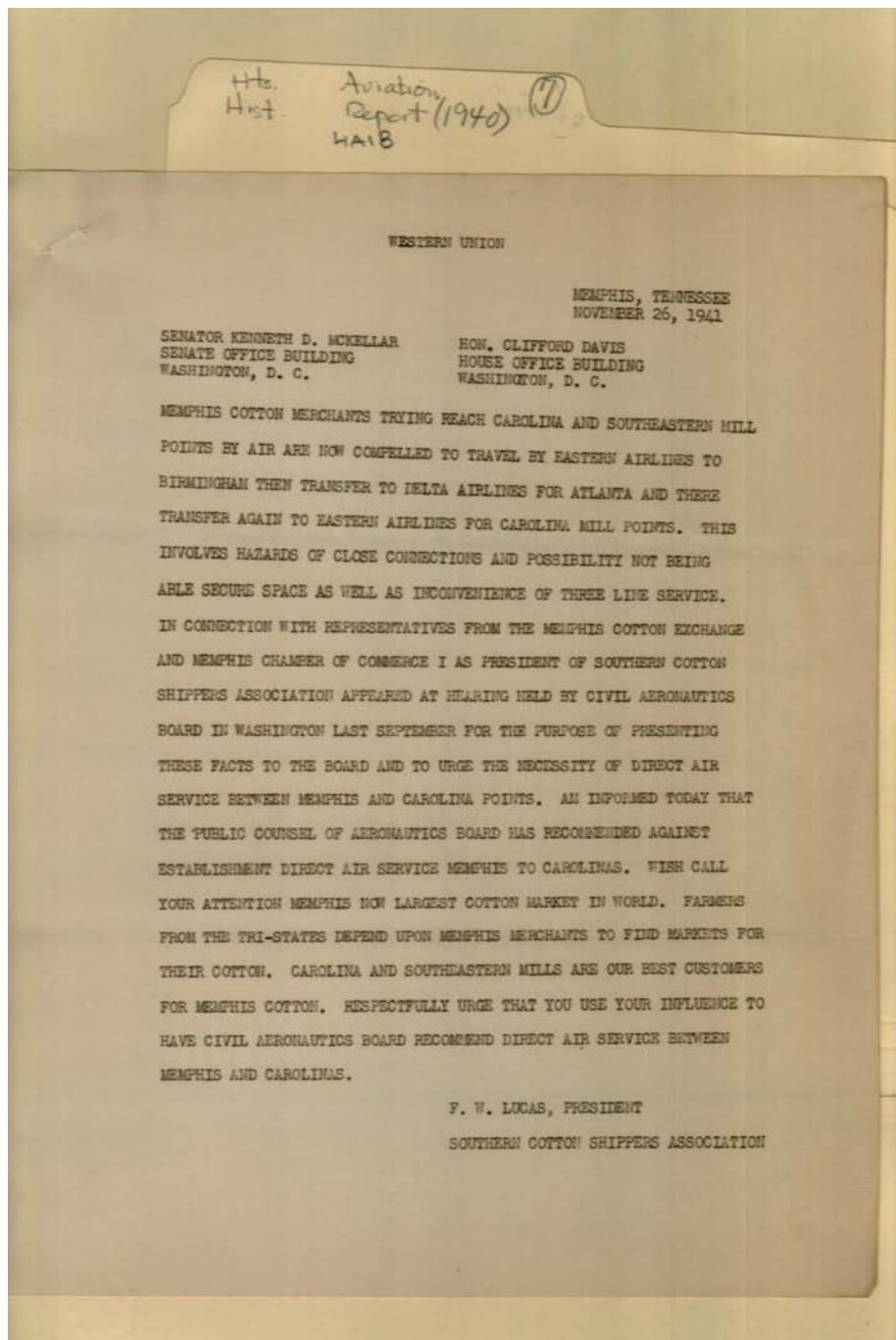
Memphis, TN

Types:

telegram

Dates:

Nov 26, 1941



Names:

Davis, Clifford
(congressman)

Lucas, F. W.
McKellar, Kenneth

Types:

telegram

Dates:

Nov 26, 1941

Hts. Aviation
Hist. Report (1940) ①
WAIB

NOVEMBER 26, 1941

HON. KENNETH MCKELLAR
SENATE OFFICE BUILDING
WASHINGTON, D. C.

HON. CLIFFORD DAVIS,
HOUSE OFFICE BUILDING
WASHINGTON, D. C.

LAST SEPTEMBER THE MEMPHIS COTTON EXCHANGE, THE MEMPHIS CHAMBER OF COMMERCE, TOGETHER WITH OTHER INTERESTED PARTIES, INCLUDING THE SOUTHERN MILLS, TESTIFIED AT A PUBLIC HEARING HELD IN WASHINGTON BEFORE THE CIVIL AERONAUTICS BOARD AND GAVE WHAT IN OUR JUDGMENT WAS CONCLUSIVE EVIDENCE AS TO THE VITAL NECESSITY OF DIRECT PLANE SERVICE BETWEEN MEMPHIS, THE WORLD'S LARGEST SPOT COTTON MARKET, AND THE CAROLINA MILLS, THE LARGEST COTTON CONSUMING SECTION IN THE UNITED STATES. THE EASTERN AIR LINES AT THIS HEARING OFFERED TO RENDER THE SERVICE AND TESTIFIED THAT IT COULD BE DONE WITHOUT THE NECESSITY OF THEIR PLACING IN USE ANY ADDITIONAL EQUIPMENT, THUS COMPLYING WITH WAR DEPARTMENT REQUESTS THAT NO ADDITIONAL EQUIPMENT BE USED BY COMMERCIAL AVIATION DURING THE PRESENT EMERGENCY. IT IS QUITE TRUE THAT WE NOW HAVE A PLANE SERVICE BETWEEN MEMPHIS AND THE CAROLINAS. TO USE THIS SERVICE IT IS NECESSARY TO TAKE A LOCAL PLANE FROM MEMPHIS TO BIRMINGHAM, TRANSFER THERE TO ATLANTA AND THEN AGAIN TO A TRANSCONTINENTAL PLANE ORIGINATING AT BROWNSVILLE, TEXAS, WITH NO ASSURANCE OF BEING ABLE TO GET ACCOMMODATIONS ON SAID TRANSCONTINENTAL FLIGHT. TIME BEING THE ALL IMPORTANT ELEMENT IN ALL COTTON TRANSACTIONS, THIS SERVICE HAS BEEN FOUND BY TEST TO BE IMPRACTICAL OF USE BY BOTH MEMPHIS SPOT COTTON INTERESTS AND THE MILL INTERESTS. WE HAVE JUST RECEIVED THE INFORMATION THAT THE PUBLIC ATTORNEY FOR THE CAB IN THE FACE OF ALL TESTIMONY GIVEN HAS TURNED IN AN ADVERSE REPORT. IN THE LIGHT OF THE TESTIMONY GIVEN AT THE HEARING WE ARE UNABLE TO UNDERSTAND WHY SUCH A REPORT WAS HANDED DOWN BY THE PUBLIC ATTORNEY AND ARE FORCED TO THE CONCLUSION THAT WE ARE BEING DISCRIMINATED AGAINST. WITH YOUR INTIMATE KNOWLEDGE OF ALL PHASES OF THE COTTON BUSINESS, WE KNOW YOU READILY SEE THE JUSTICE OF OUR POSITION AND WE, THEREFORE, LAY THE ENTIRE MATTER BEFORE YOU WITH THE EARNEST REQUEST THAT YOU GIVE IT YOUR CONSIDERATION AND SUPPORT.

A. H. BOWER, SECRETARY
MEMPHIS COTTON EXCHANGE

Names:

Bower, A. H.,
Cotton Exchange

Davis, Clifford
(congressman)

McKellar, Kenneth
Public Hearing

Places:

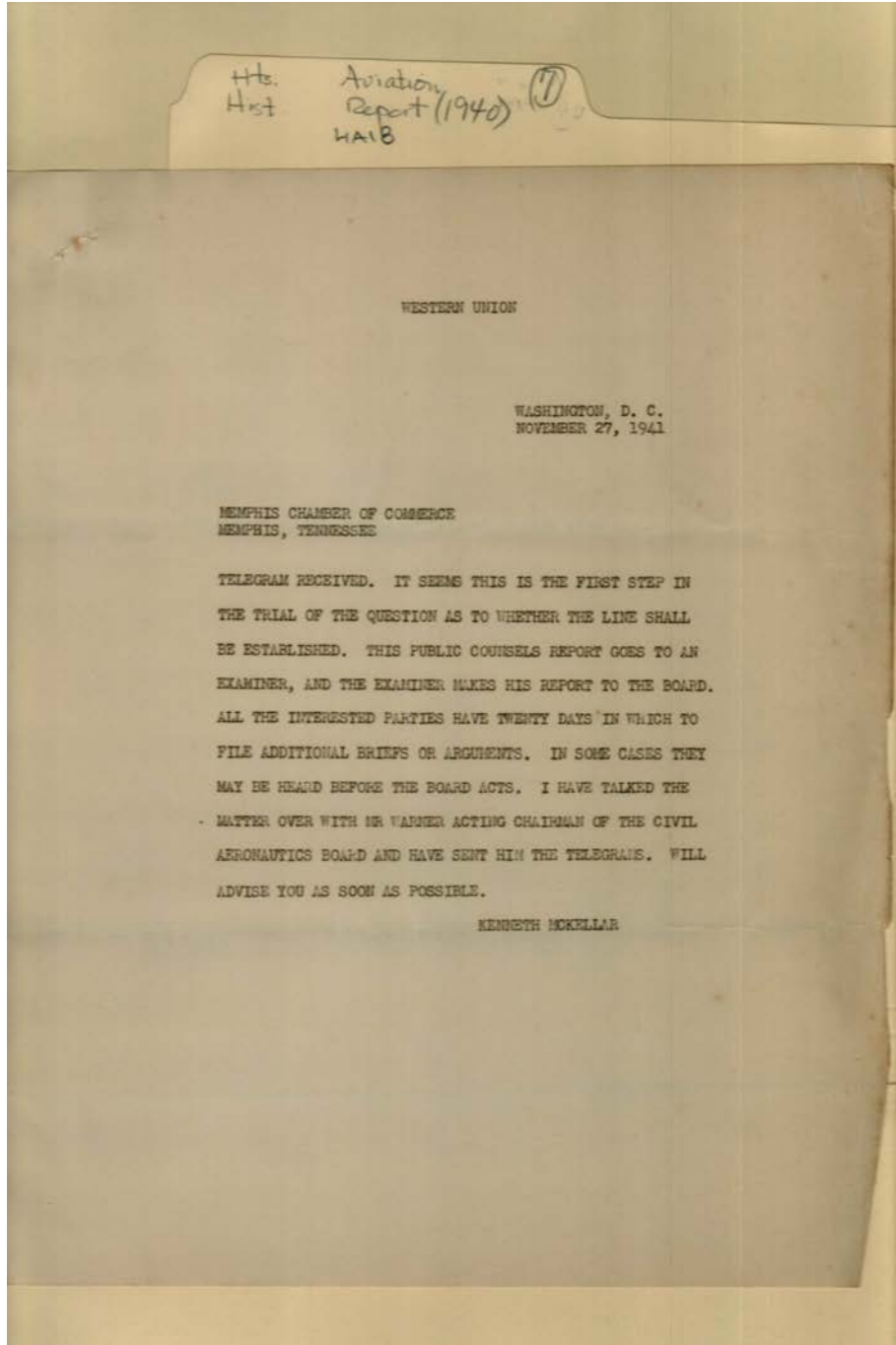
Memphis, TN

Types:

correspondence

Dates:

Nov 26, 1941



Names:

McKellar, Kenneth

Memphis Chamber of
Commerce

Varner,

Places:

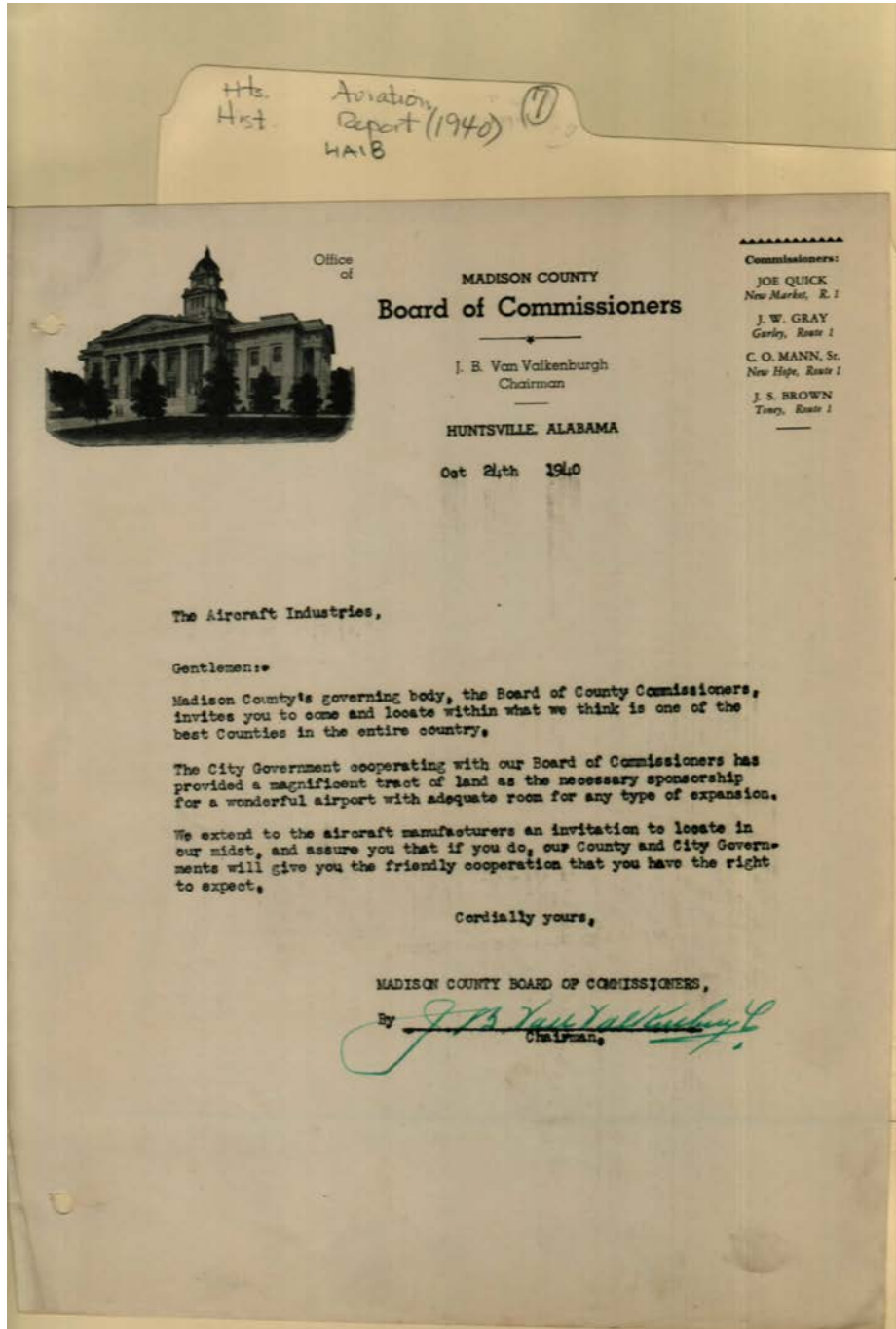
Washington, DC

Types:

telegram

Dates:

Nov 27, 1941



Names:

Brown, J. S.
Gray, J. W.

Mann, C. O.
Quick, Joe

Van Valkenburgh, J.
B.

Places:

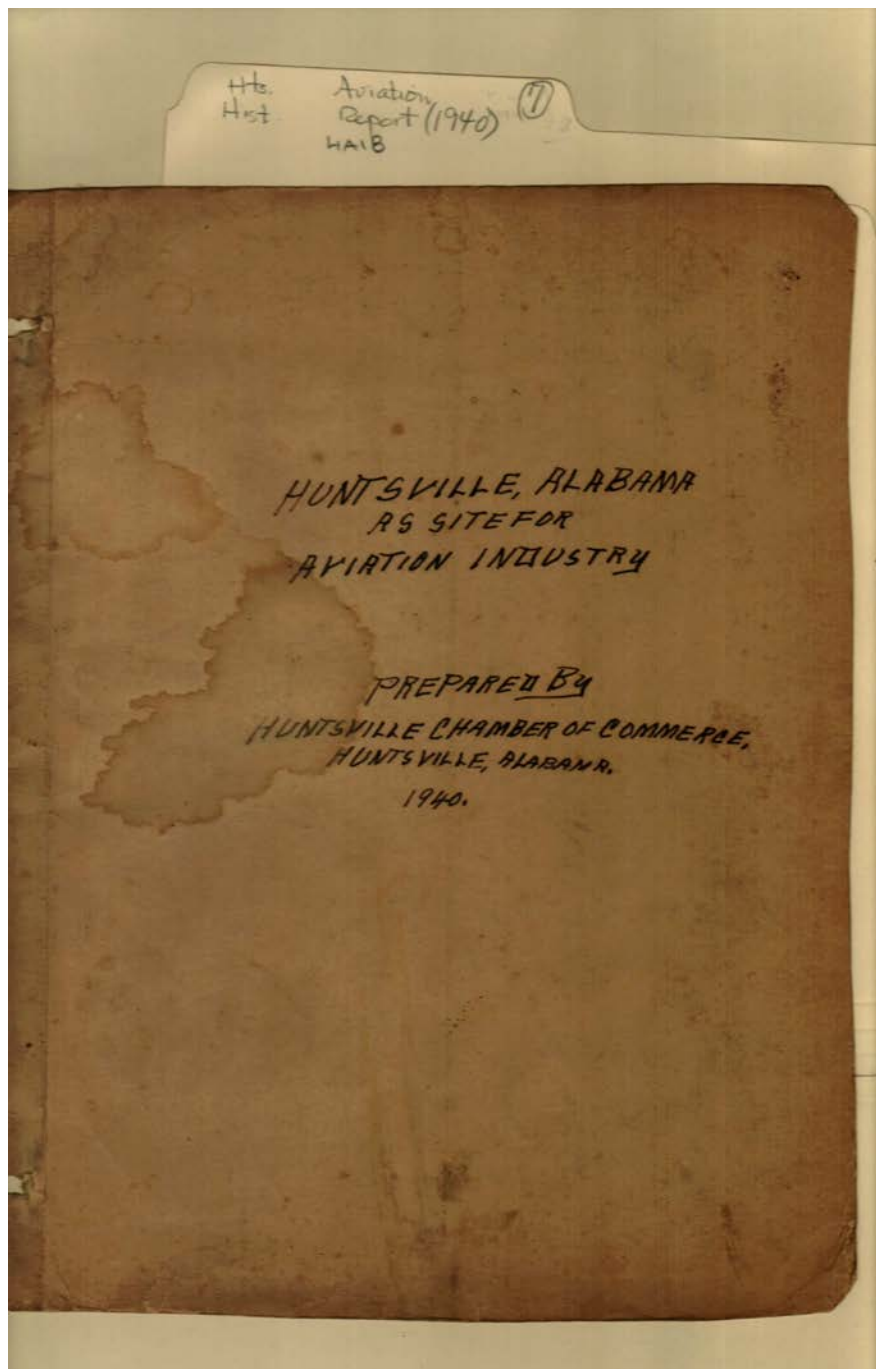
Huntsville, AL

Types:

correspondence

Dates:

Oct 24, 1940



Names:

Huntsville Site for
Aviation Industry

Places:

Huntsville, AL

Types:

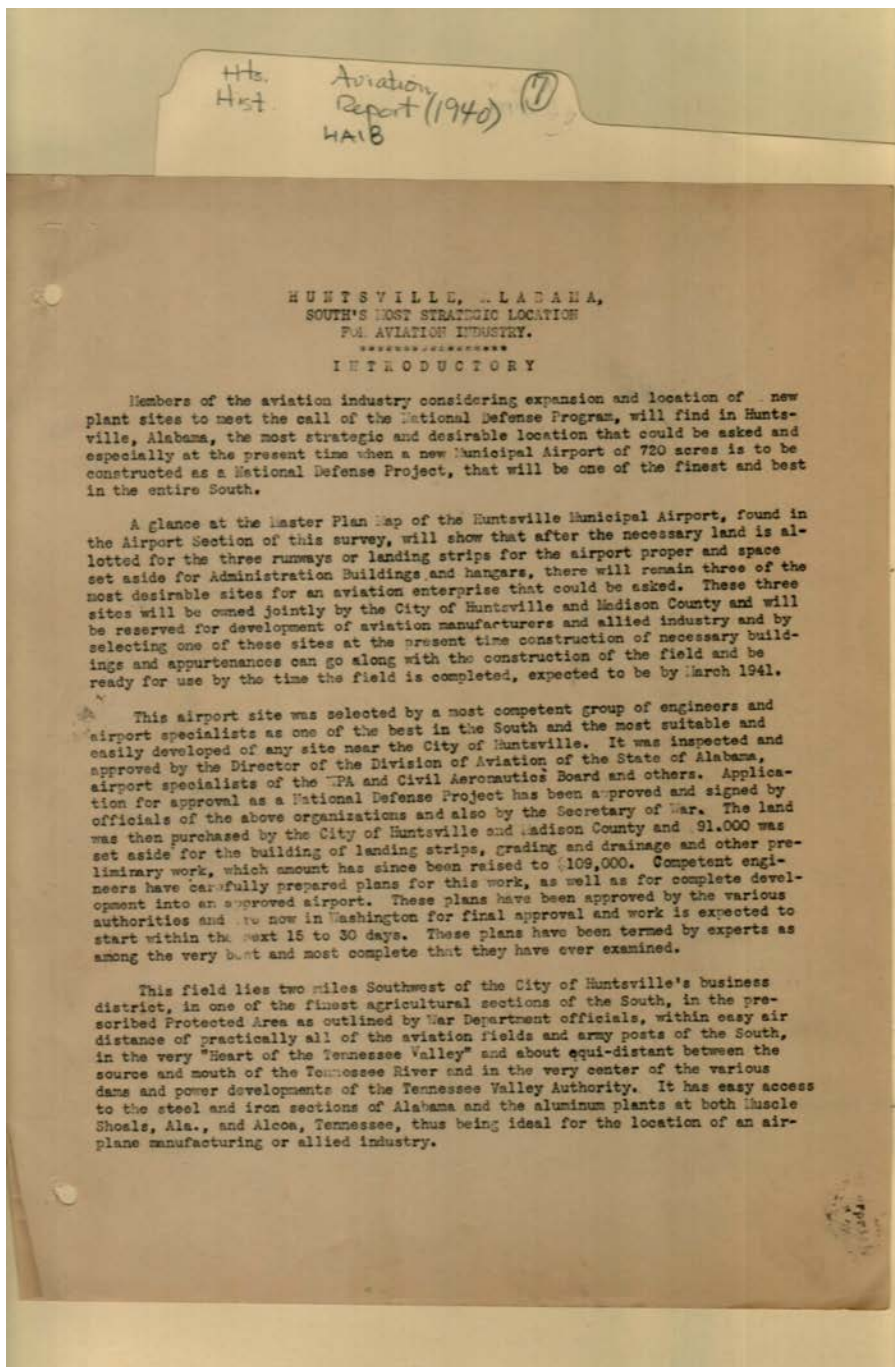
cover

Dates:

1940

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7
Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 46 r04a18-07-000-0111 [Contents](#) [Index](#) [About](#)

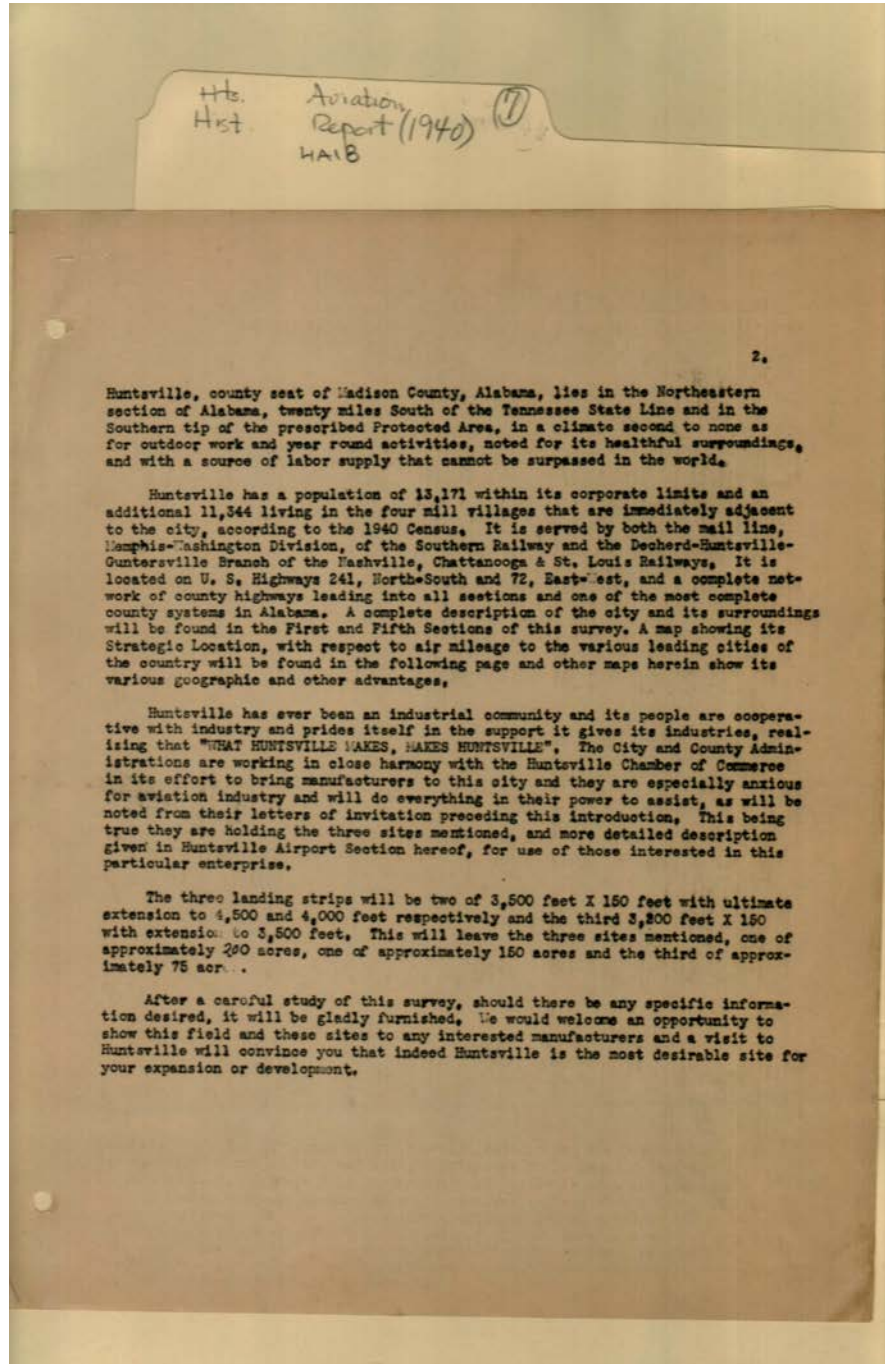


Names:

Introduction

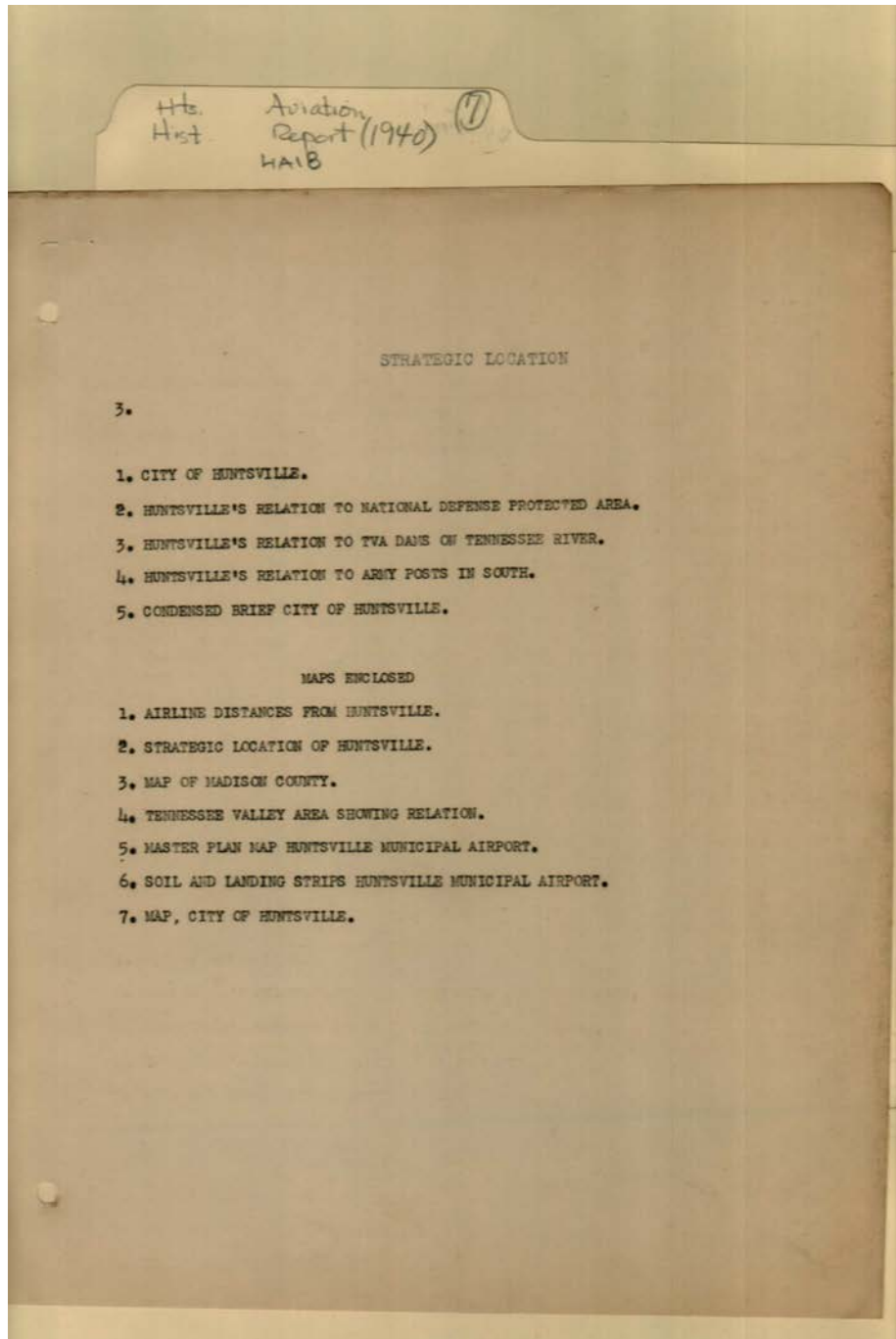
Types:

brief



Names:
Huntsville
Information

Types:
brief



Names:

City of Huntsville

Maps included

Places:

Huntsville, AL

Types:

contents



Names:

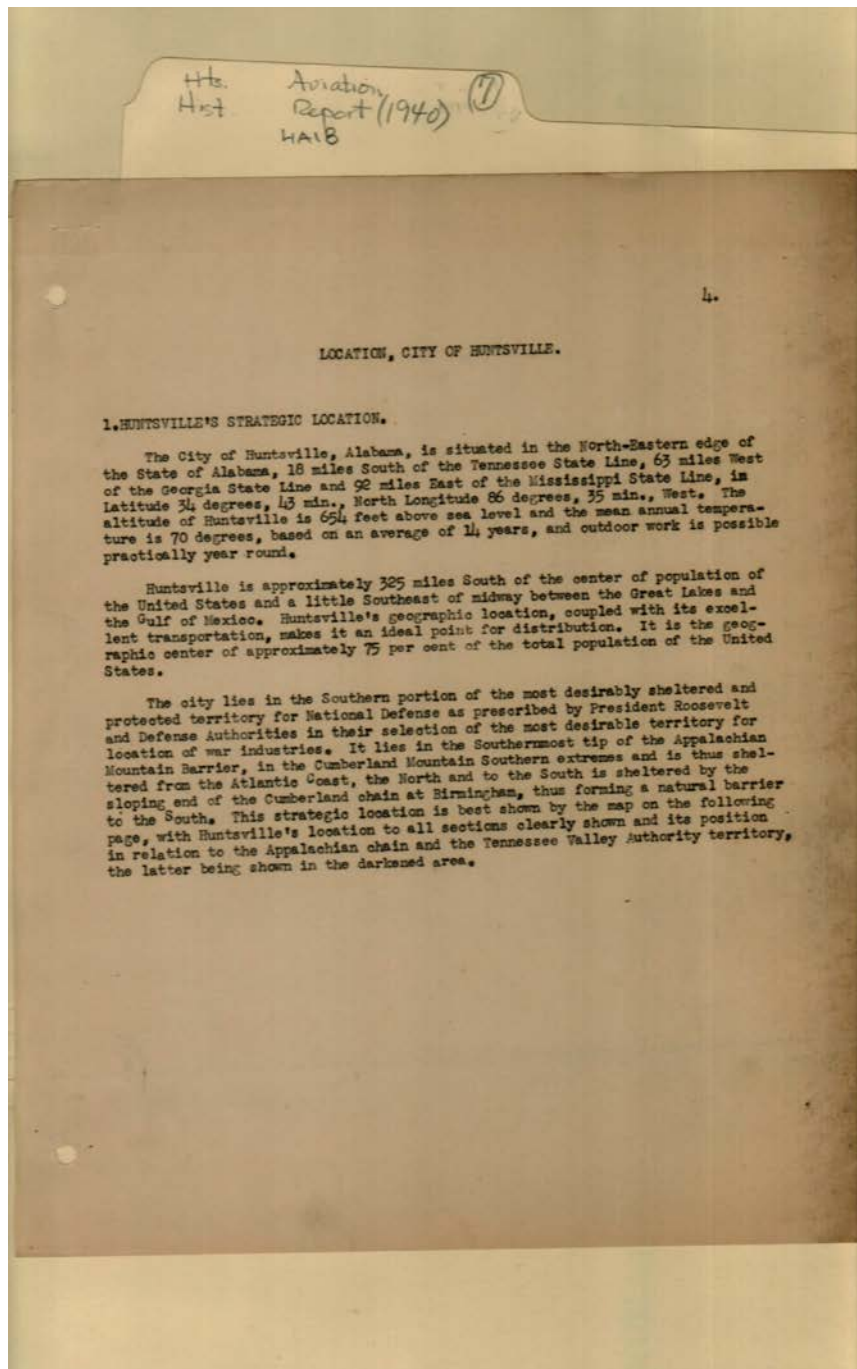
Distance from
Huntsville to Coast

Types:

map

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7
Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 50 r04a18-07-000-0115 [Contents](#) [Index](#) [About](#)

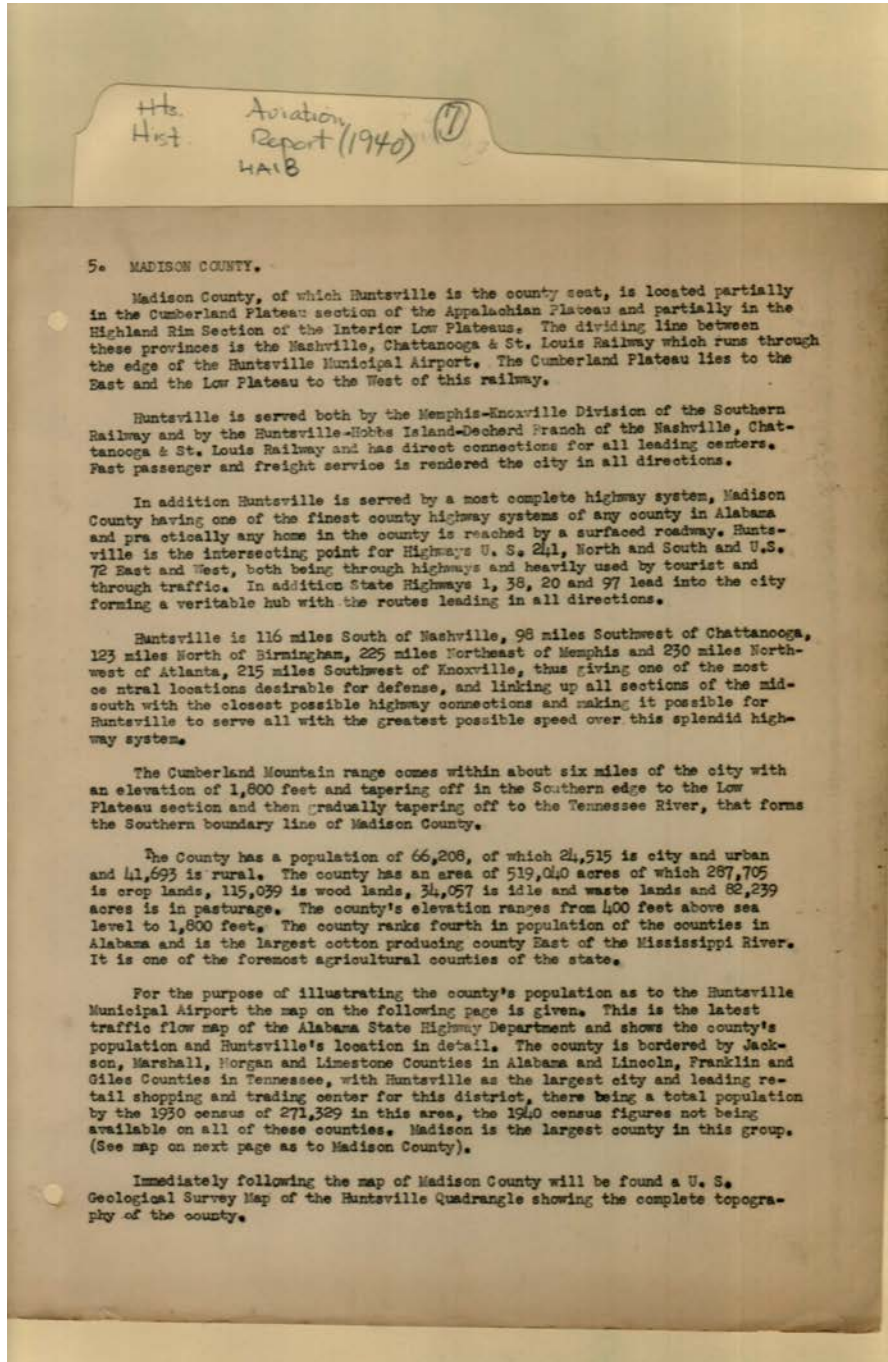


Names:

Strategic Location

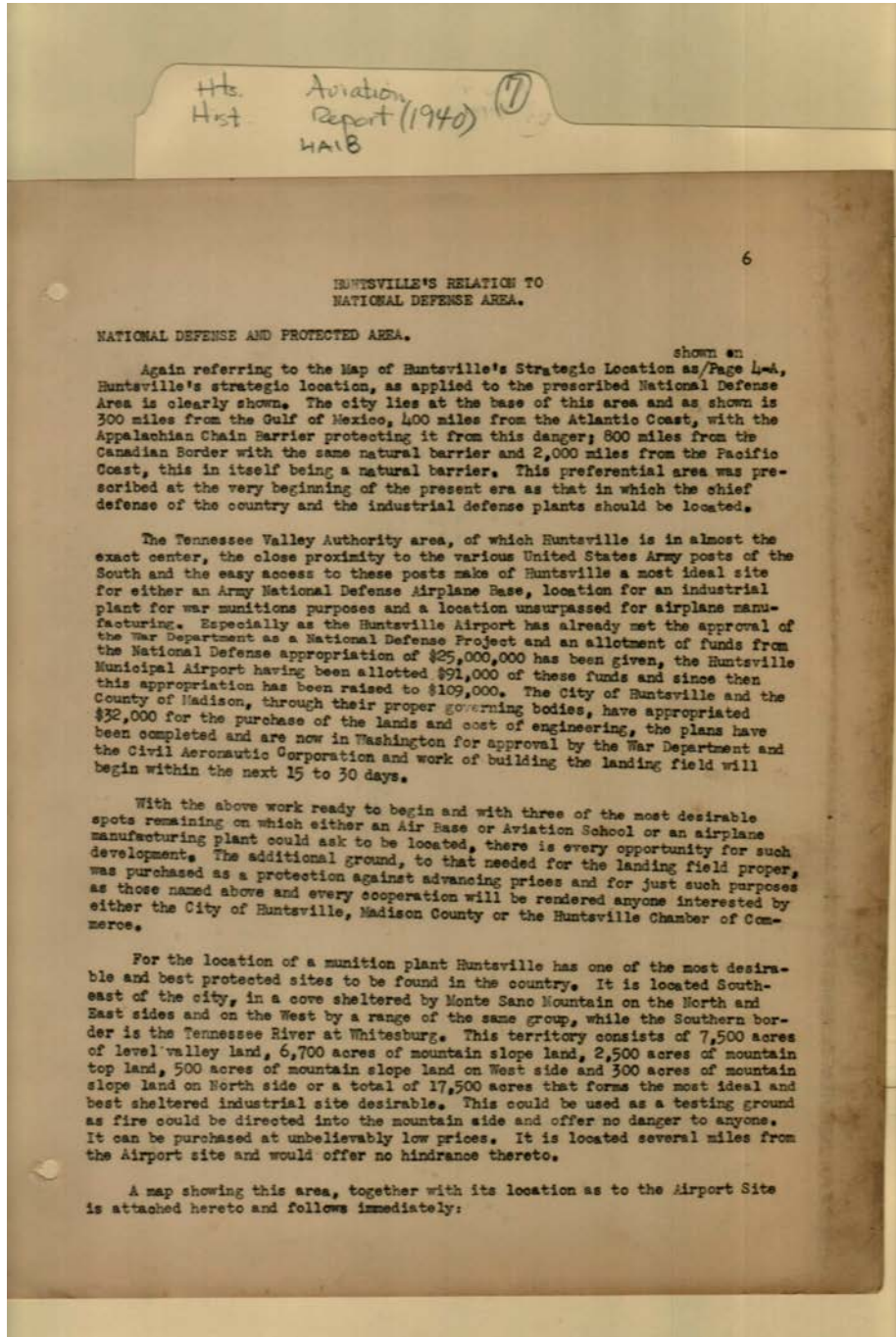
Types:

brief



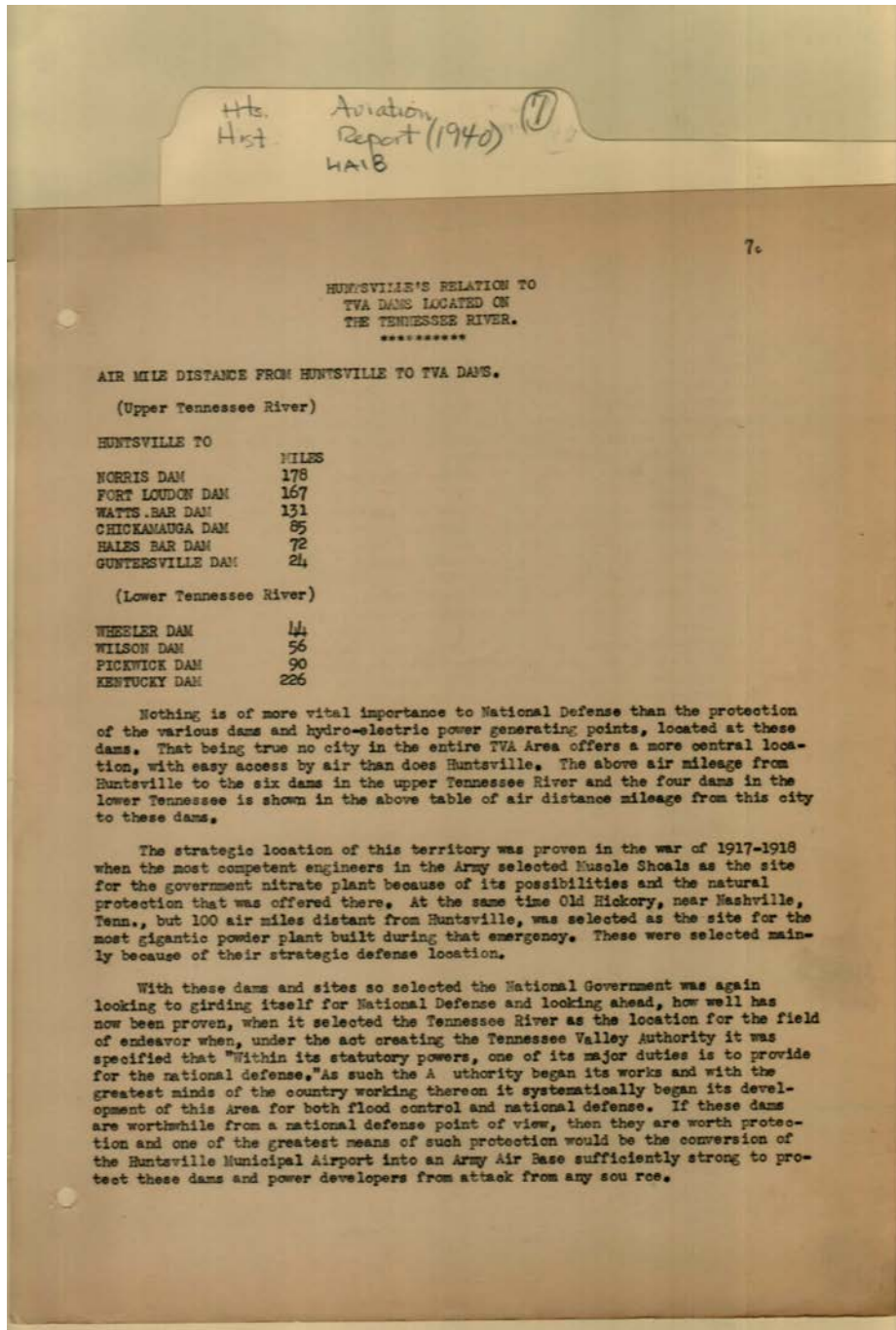
Names:
Madison County

Types:
brief



Names:
National Defense
Area

Types:
brief



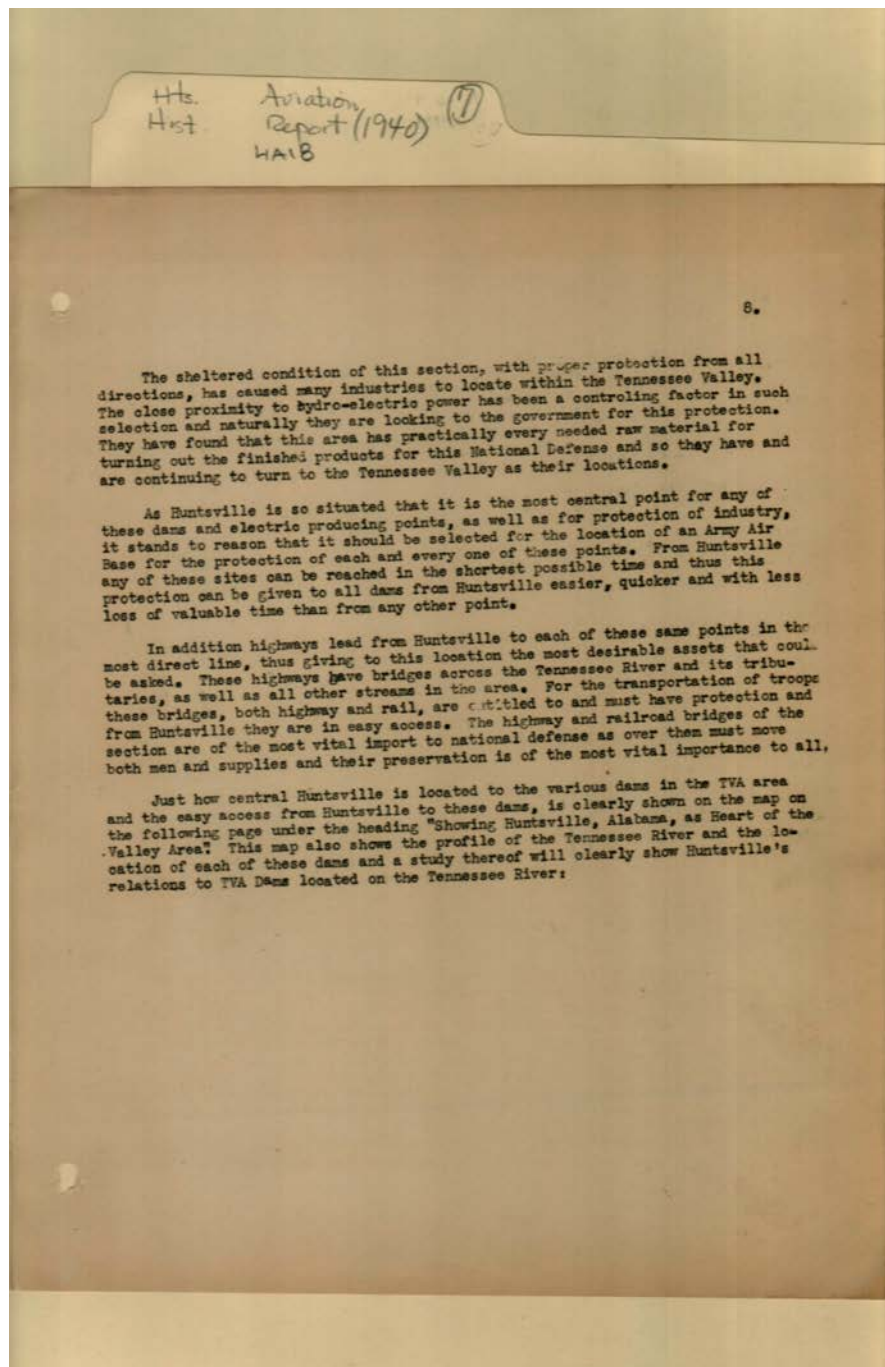
Names:
Strategic Location

Places:
Huntsville, AL

Types:
brief

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7
Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 54 r04a18-07-000-0119 [Contents](#) [Index](#) [About](#)



Names:

Huntsville Location

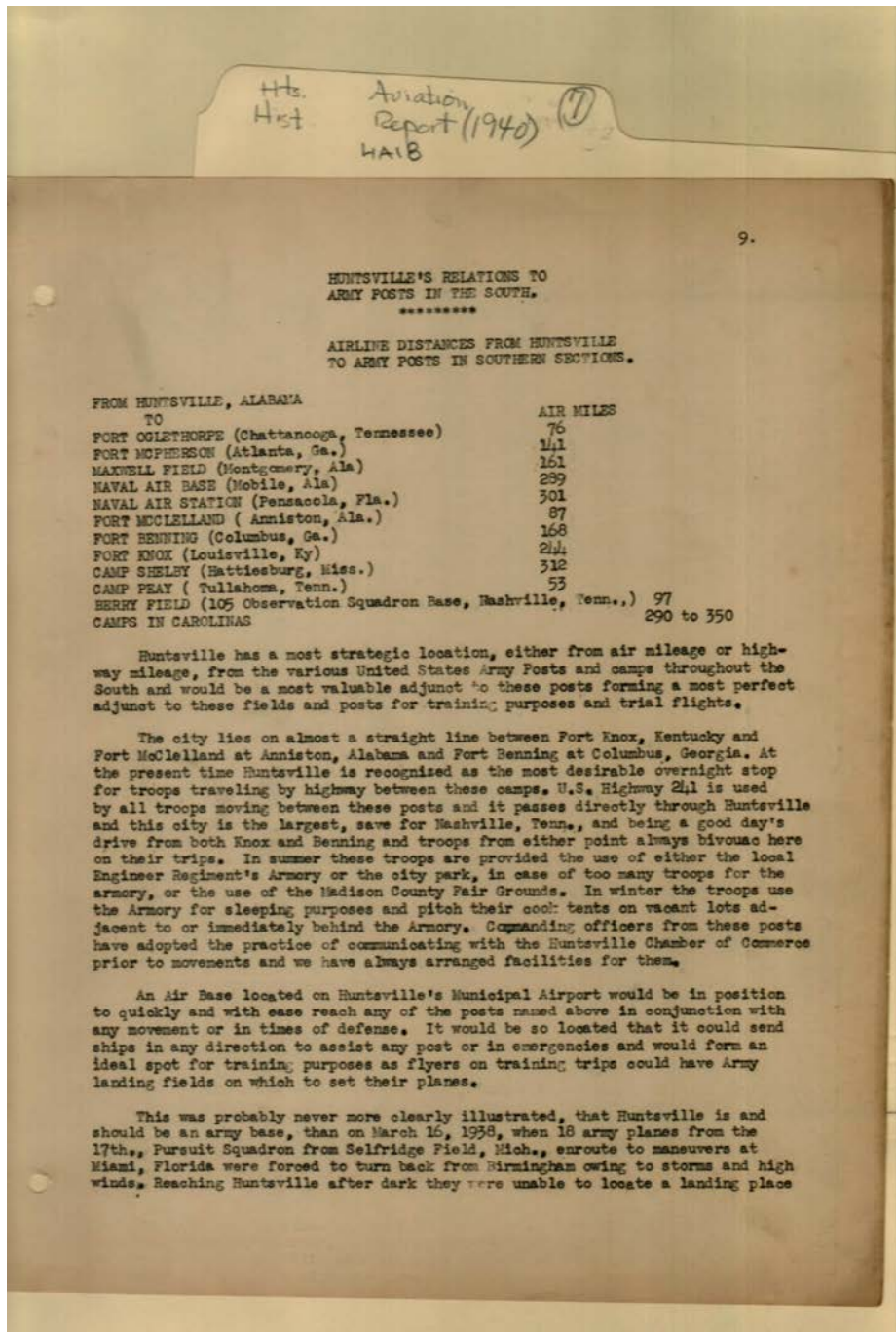
Types:

brief



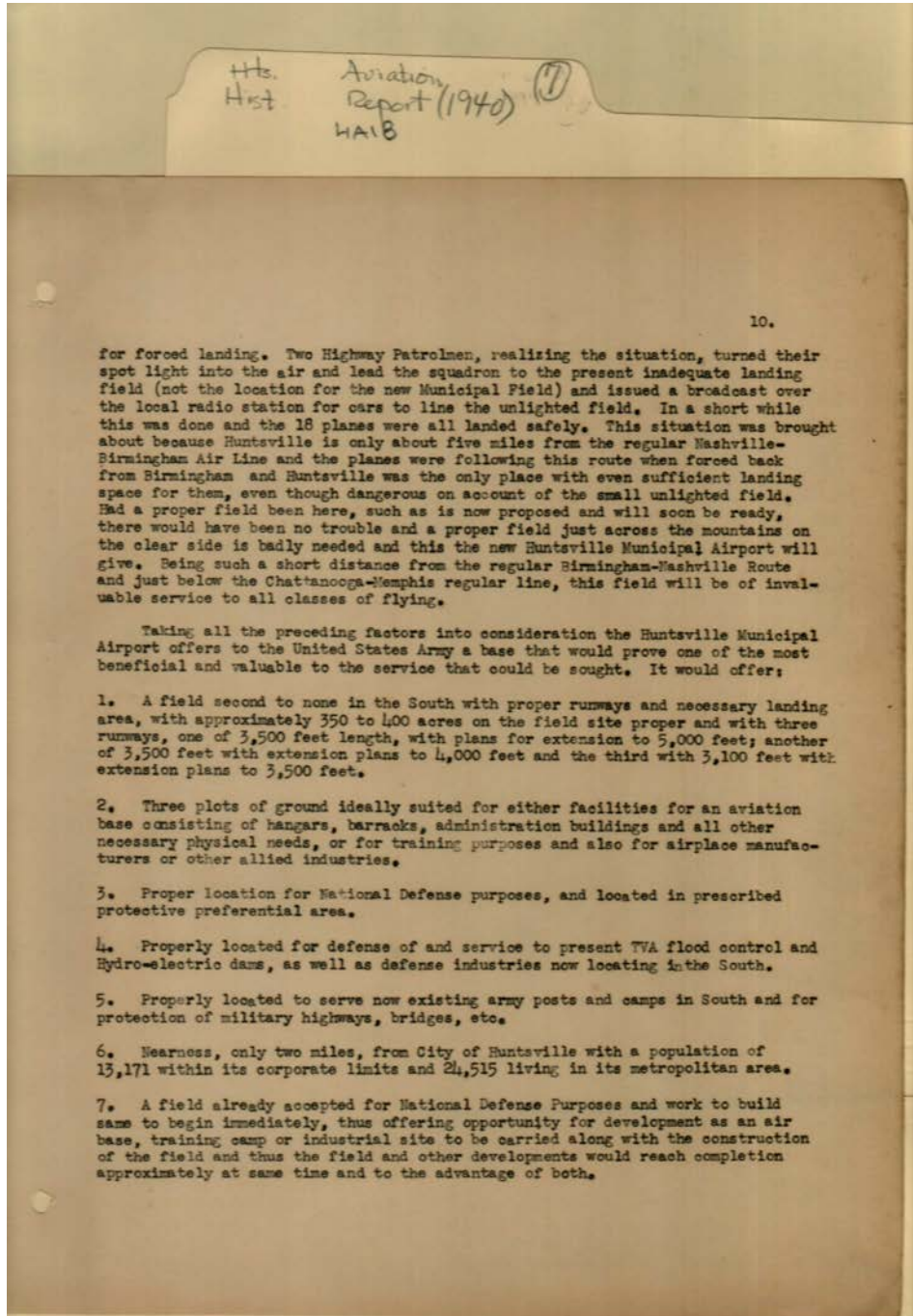
Names:
Tennessee Valley
Area

Types:
map



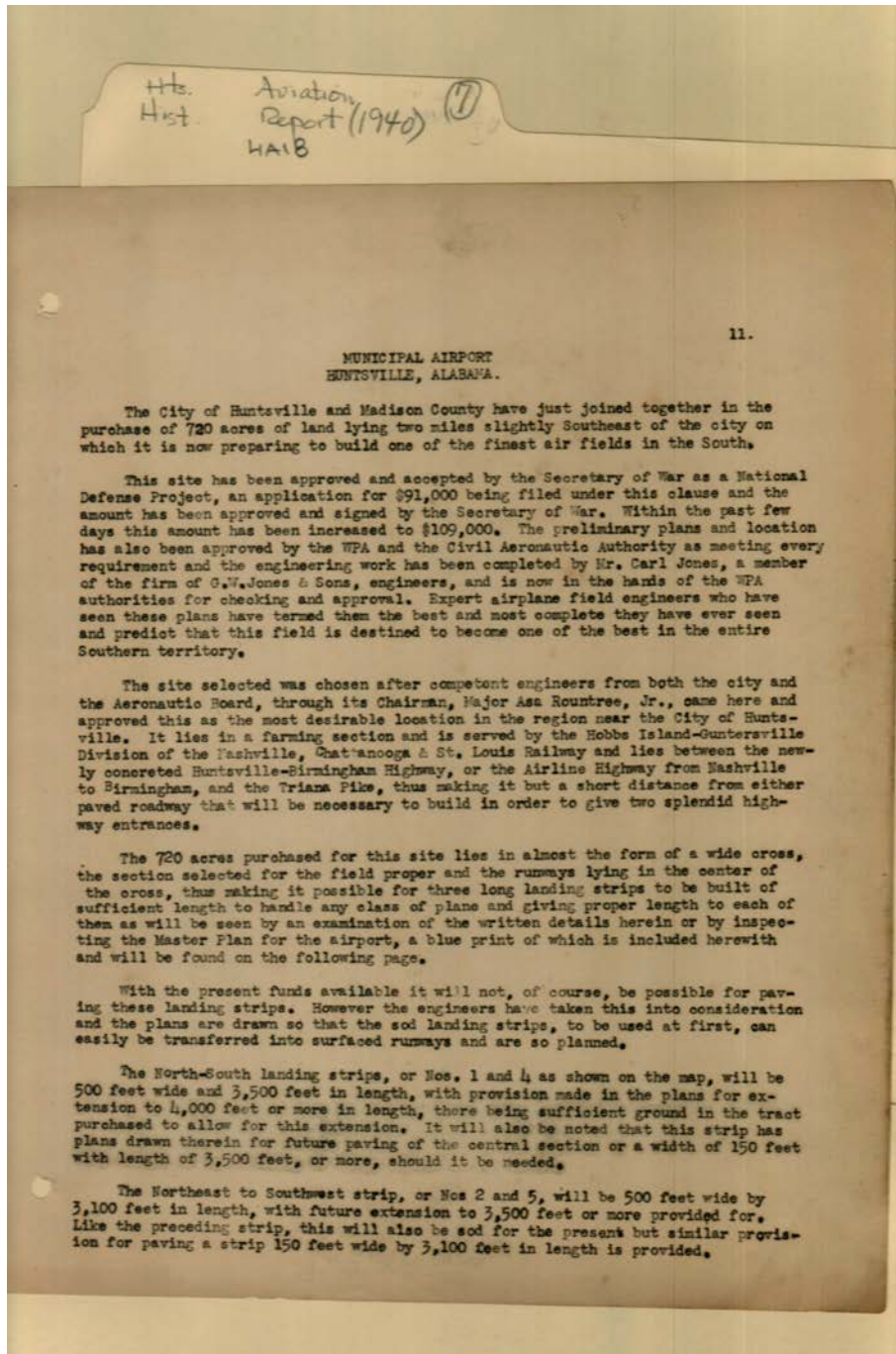
Names:
Army Post in
Huntsville

Types:
brief



Names:
About Huntsville

Types:
brief



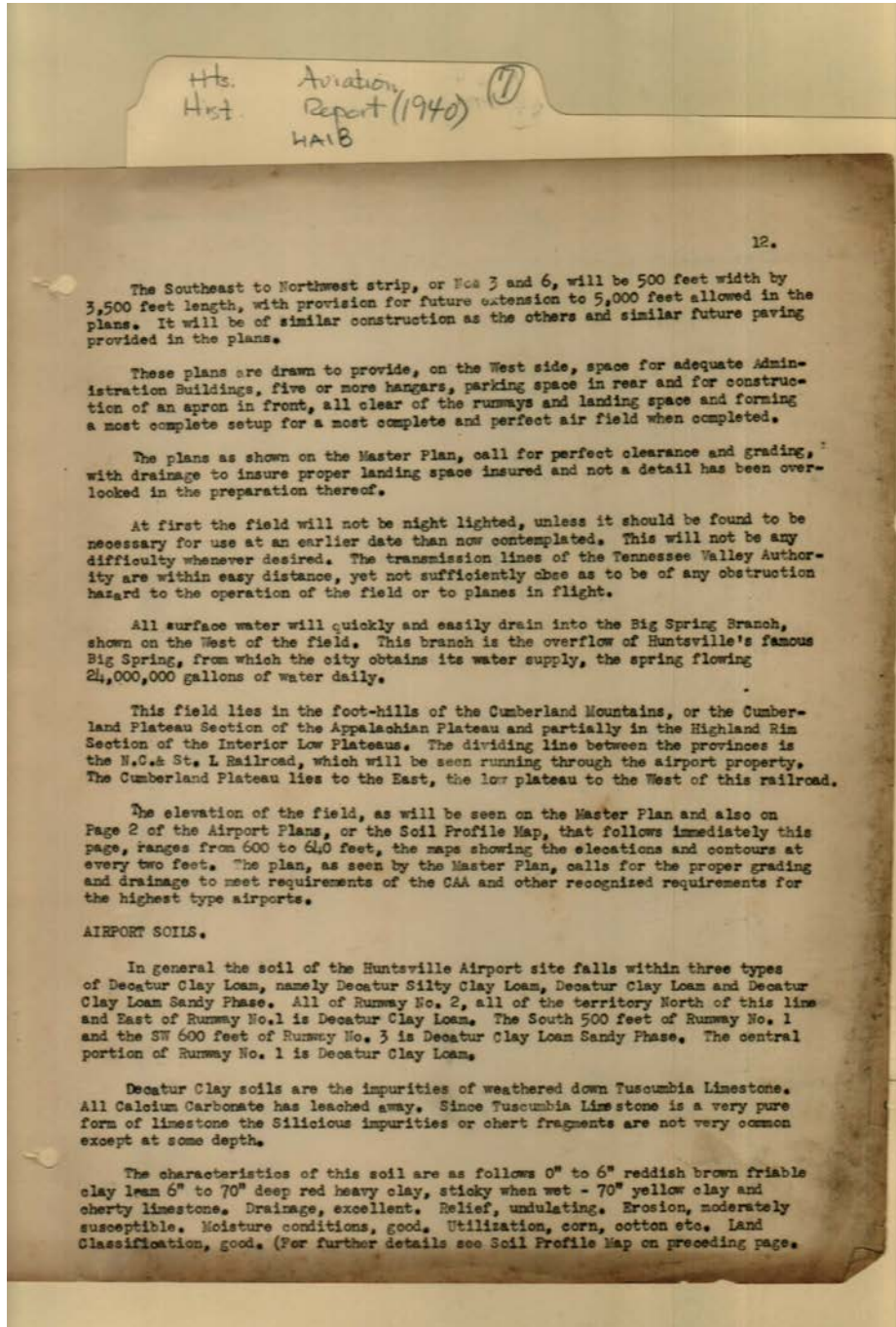
Names:

Huntsville Municipal
Airport

Rountree, Asa, Jr.

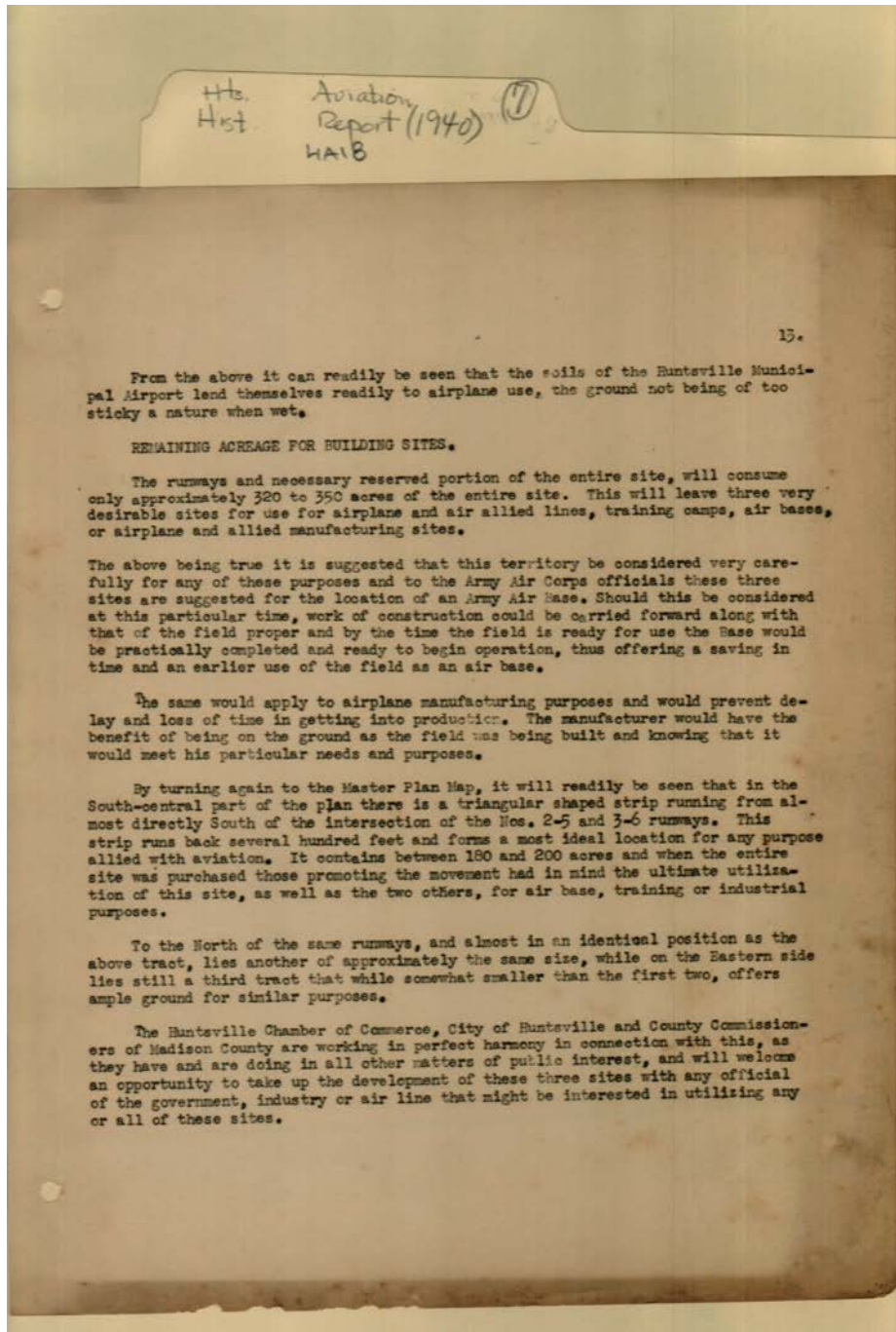
Types:

brief



Names:
Airport Soils

Types:
brief

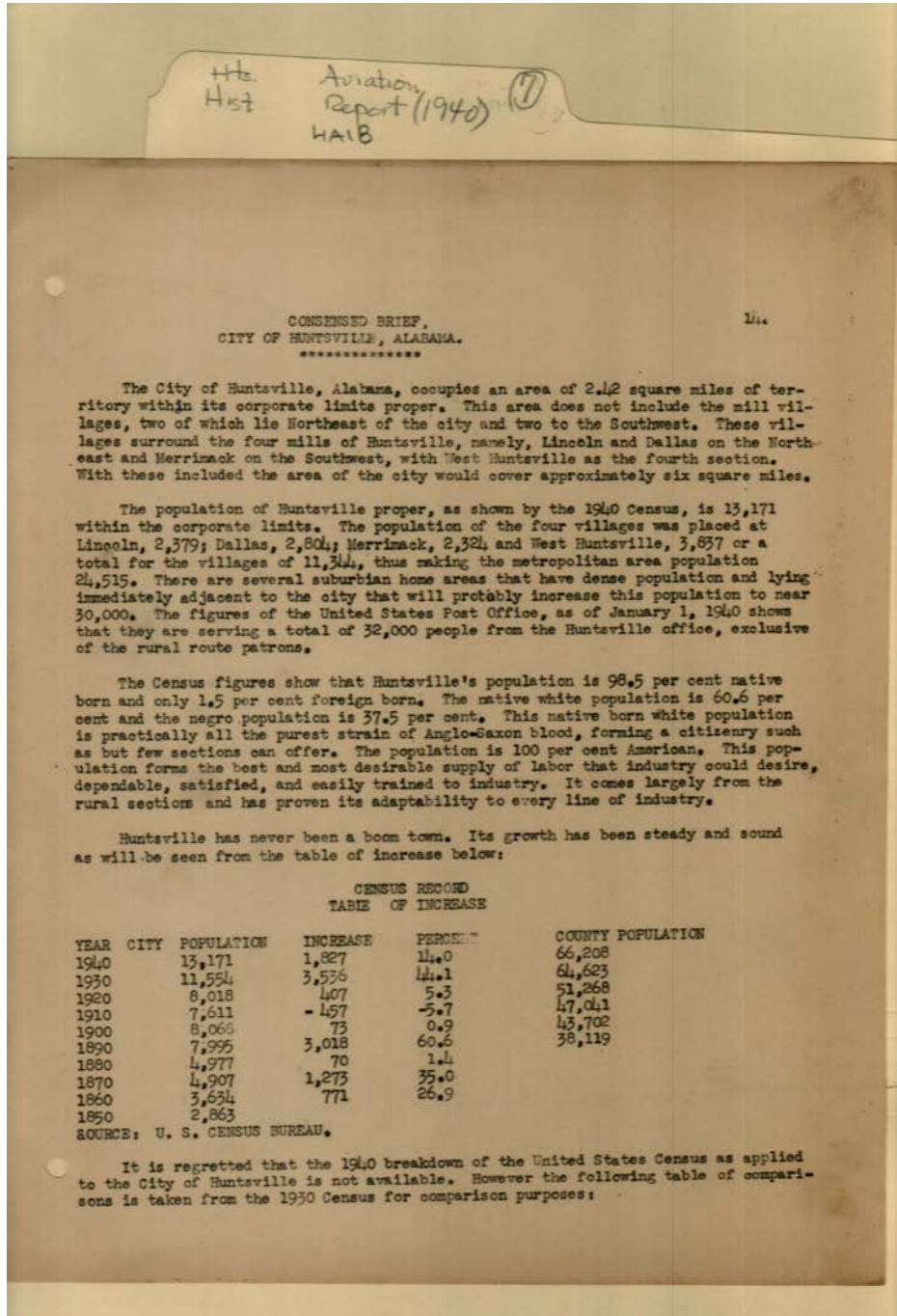


Names:

Remaining Acreage
for Building

Types:

brief



Names:
 Condensed Brief

Places:
 Huntsville, AL

Types:
 brief

Hts. Aviation Report (1940) ①
 Hist. HA18

15.

**HUNTSVILLE'S POPULATION BY CLASSES,
 RACES, SEX AND AGE, 1930 CENSUS.**

AGE & YEARS	TOTAL	ALL CLASSES		NATIVE WHITE		FOREIGN BORN		NEGRO	
		MALE	FEMALE	MALE	FEMALE	MALE	FEMALE	MALE	FEMALE
HUNTSVILLE	11,554-5,338	6,216	3,611	4,026	53	39	1,674	2,751	
Under 5 yrs.	1,021	526	495	380	351		146	144	
5 to 9	1,127	558	569	384	381		174	138	
10 to 14	994	496	498	343	346	2	151	152	
15 to 19	1,061	474	587	309	377	1	164	203	
20 to 24	1,212	491	721	337	469	2	154	250	
25 to 29	1,062	478	584	329	367	5	144	215	
30 to 34	846	387	459	262	300	6	119	155	
35 to 40	1,534	677	857	471	550	11	111	195	
45 to 54	1,357	655	702	416	451	11	10	229	
55 to 64	790	372	415	232	243	11	4	132	
65 to 74	356	156	200	114	137	5	4	37	
75 & Over	181	57	124	30	73	2	3	25	
Unknown	13	8	5	4	1		4	4	

SOURCE: U. S. CENSUS BUREAU, 1930, ALABAMA.

ALTITUDE AND CLIMATE.

Huntsville's altitude is 654 feet above sea level, with one of the most even climates of any city in the South, being neither too hot in summer nor too cold in winter and outdoor work is possible almost year round. The mean annual temperature is 70 degrees, based on an average of the past 16 years; the highest temperature being 108 and the lowest ever recorded being 8 degrees below zero and that only on one occasion. It is very seldom that the temperature ever reaches zero and even freezing or below is recorded but seldom during a season. The chart on the next page gives the average for each month at 6 A.M., for sixteen year period.

Names:
 Huntsville Population
 by Classes

Places:
 Huntsville, AL

Types:
 list

Dates:
 1930

Hts. Aviation Report (1940) ①
 Hist. HA18

16.

TABLE
 CLIMATIC DATA ON HUNTSVILLE, ALABAMA,
 16 YEAR PERIOD, 1925 - 1940

YEAR	JAN.	FEB.	MAR.	APR.	MAY	JUNE	JULY	AUG.,	SEPT.,	OCT.	NOV.	DEC.	AVE.
1925	38	42	44	56	58	72	71	68	71	54	45	34	
1926	33	38	36	45	56	65	69	69	68	54	38	39	
1927	38	47	46	56	61	69	69	65	63	51	49	36	
1928	33	33	41	47	57	69	72	70	60	56	43	35	
1929	38	33	47	55	62	68	71	69	66	50	46	35	
1930	35	42	40	53	63	67	72	67	67	49	43	33	
1931	33	38	38	49	57	69	73	66	65	51	46	48	
1932	44	45	39	53	61	72	74	73	65	50	39	39	
1933	42	38	43	50	65	65	67	65	63	49	39	42	
1934	36	28	40	51	62	69	73	71	62	51	42	34	
1935	36	36	49	53	61	65	69	67	58	47	41	26	
1936	32	34	46	49	62	70	73	70	68	54	41	41	
1937	50	38	40	51	60	71	69	71	62	51	42	36	
1938	37	44	51	53	63	68	72	71	62	50	42	36	
1939	39	41	47	51	61	71	72	68	65	53	38	36	
1940	21	37	43	51	61	67	74	72	68				
Average	36	38	43	51	60	67	71	68	64	58	52	36	53.6

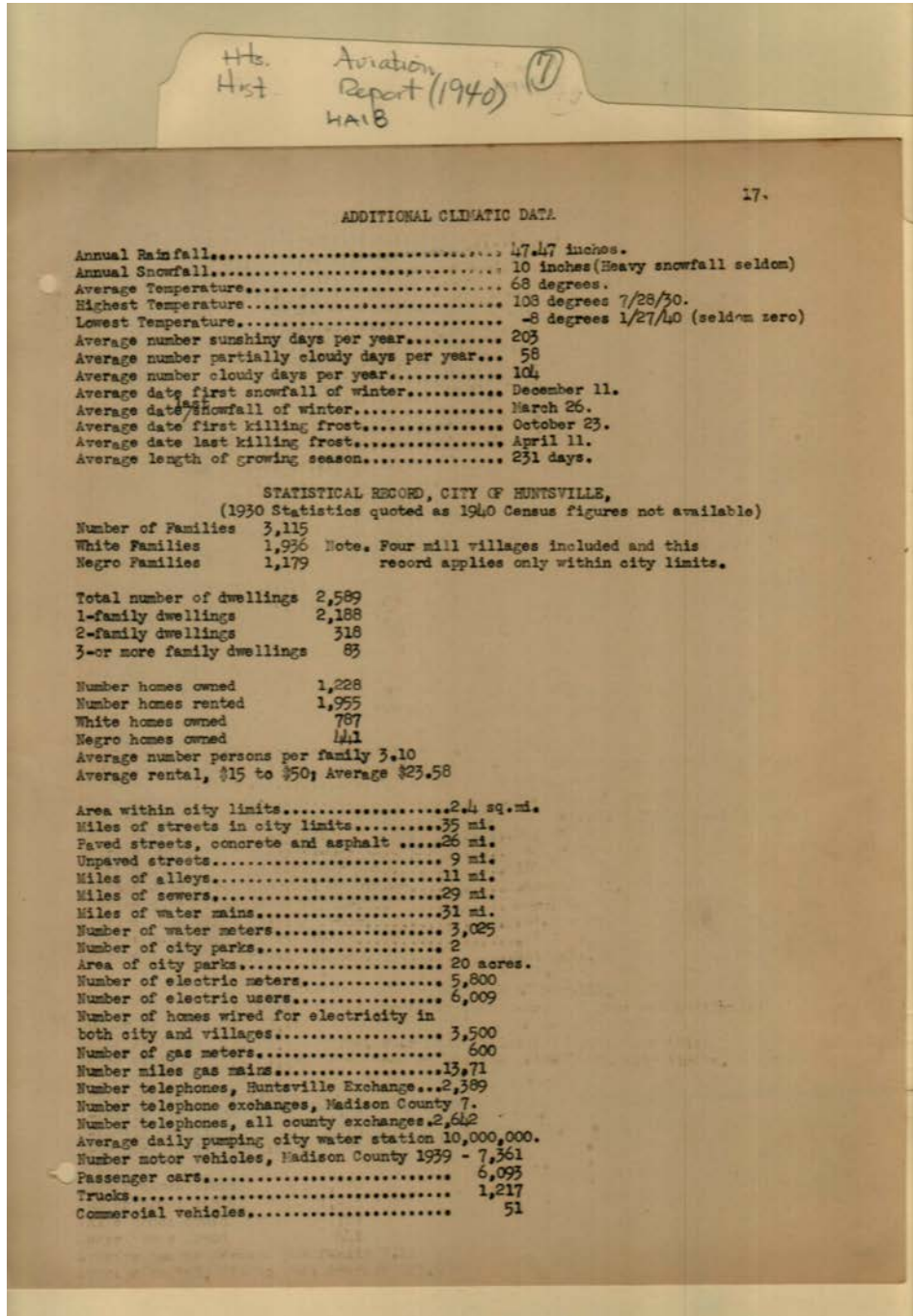
YEAR	JAN.	FEB.	MAR.	APR.	MAY	JUNE	JULY	AUG.,	SEPT.,	OCT.	NOV.	DEC.	AVE.
1925	45	50	57	69	74	86	84	85	86	60	49	41	
1926	41	47	48	62	73	79	82	78	79	63	46	45	
1927	43	53	56	67	74	78	82	80	78	65	56	43	
1928	40	45	54	60	72	78	85	82	73	66	51	44	
1929	44	43	59	66	71	81	82	85	74	60	50	43	
1930	42	53	53	70	74	83	89	84	77	61	51	39	
1931	43	49	49	65	71	86	84	79	82	65	56	53	
1932	50	55	51	65	71	81	84	83	75	61	47	43	
1933	50	45	54	62	77	81	79	78	76	61	48	49	
1934	43	40	51	64	76	83	84	82	73	63	52	40	
1935	42	45	61	64	73	78	82	79	71	60	47	33	
1936	40	43	59	63	80	89	84	85	79	65	49	47	
1937	54	45	53	66	73	84	85	85	75	60	49	43	
1938	43	53	62	66	75	80	82	84	75	67	52	44	
1939	47	51	60	64	73	82	84	82	79	63	49	45	
1940	29	44	54	63	71	81	83	82	78				
Average	43	47	55	64	73	81	83	78	76	62	50	43	62.9

Names:
 Climatic Data

Places:
 Huntsville, AL

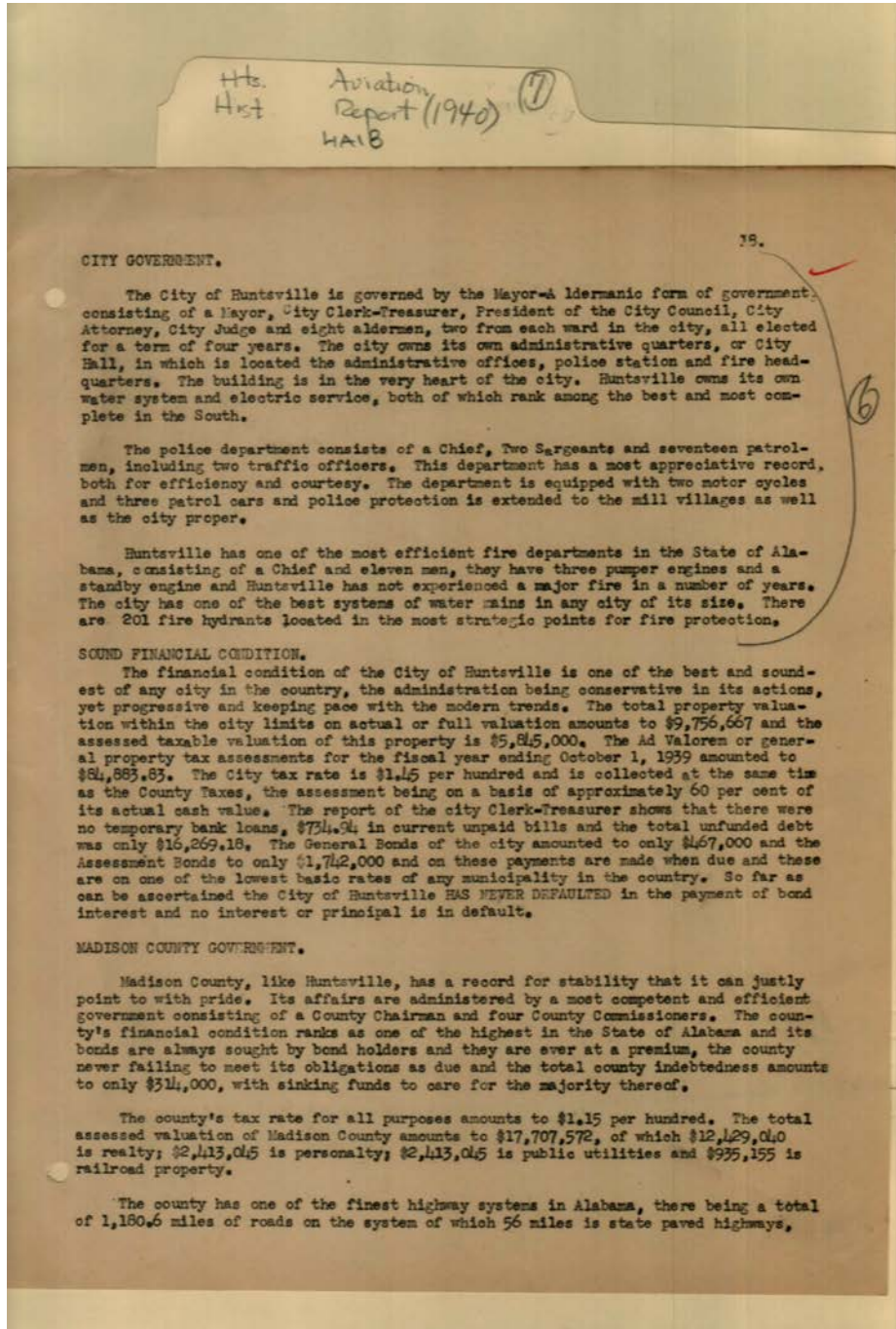
Types:
 chart

Dates:
 1940



Names:
 Climatic Data

Types:
 brief



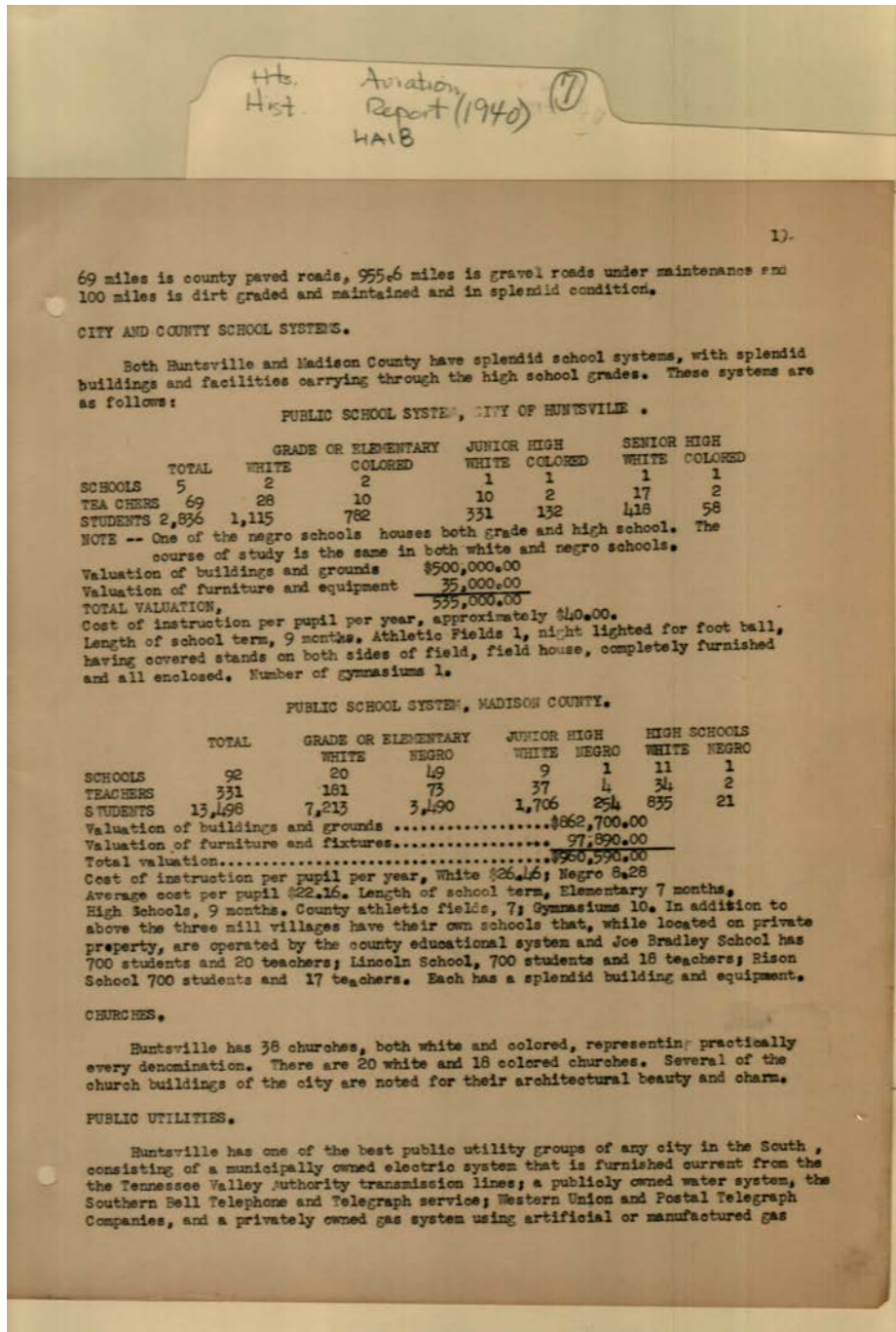
Names:

Huntsville
Government

Madison County
Government

Types:

brief



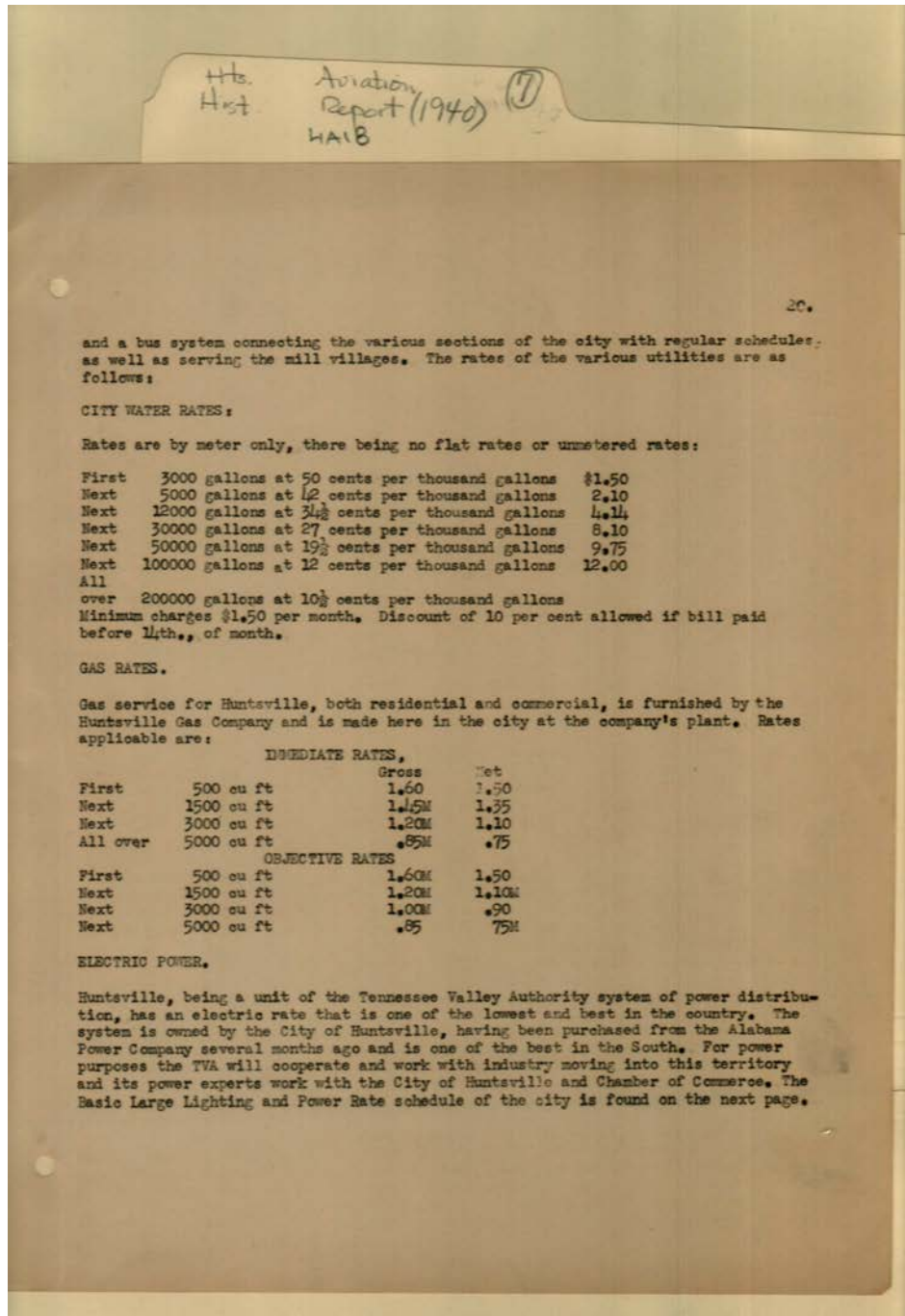
Names:

Public Utilities

School Systems

Types:

brief



Names:

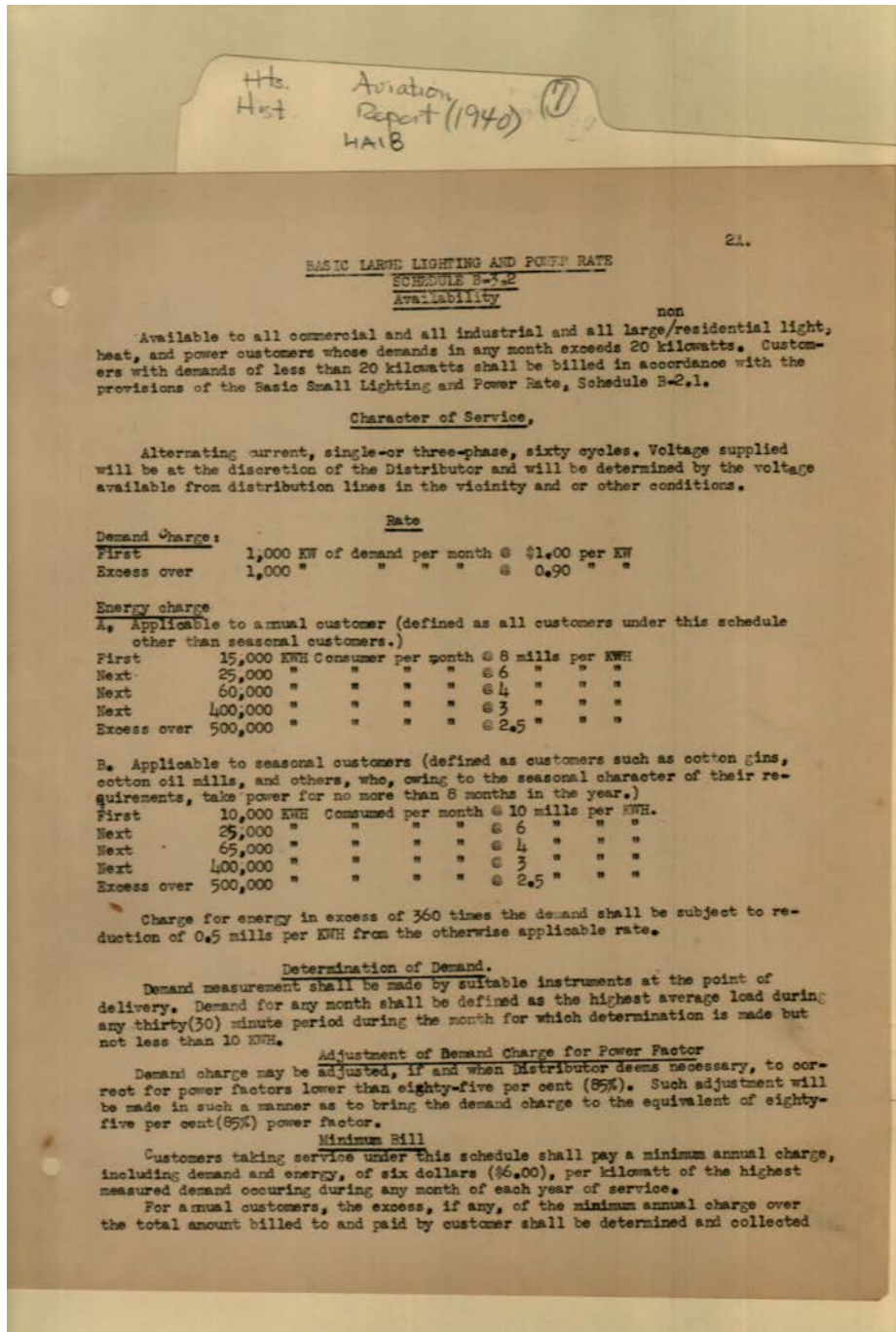
Water and Gas Rates

Places:

Huntsville, AL

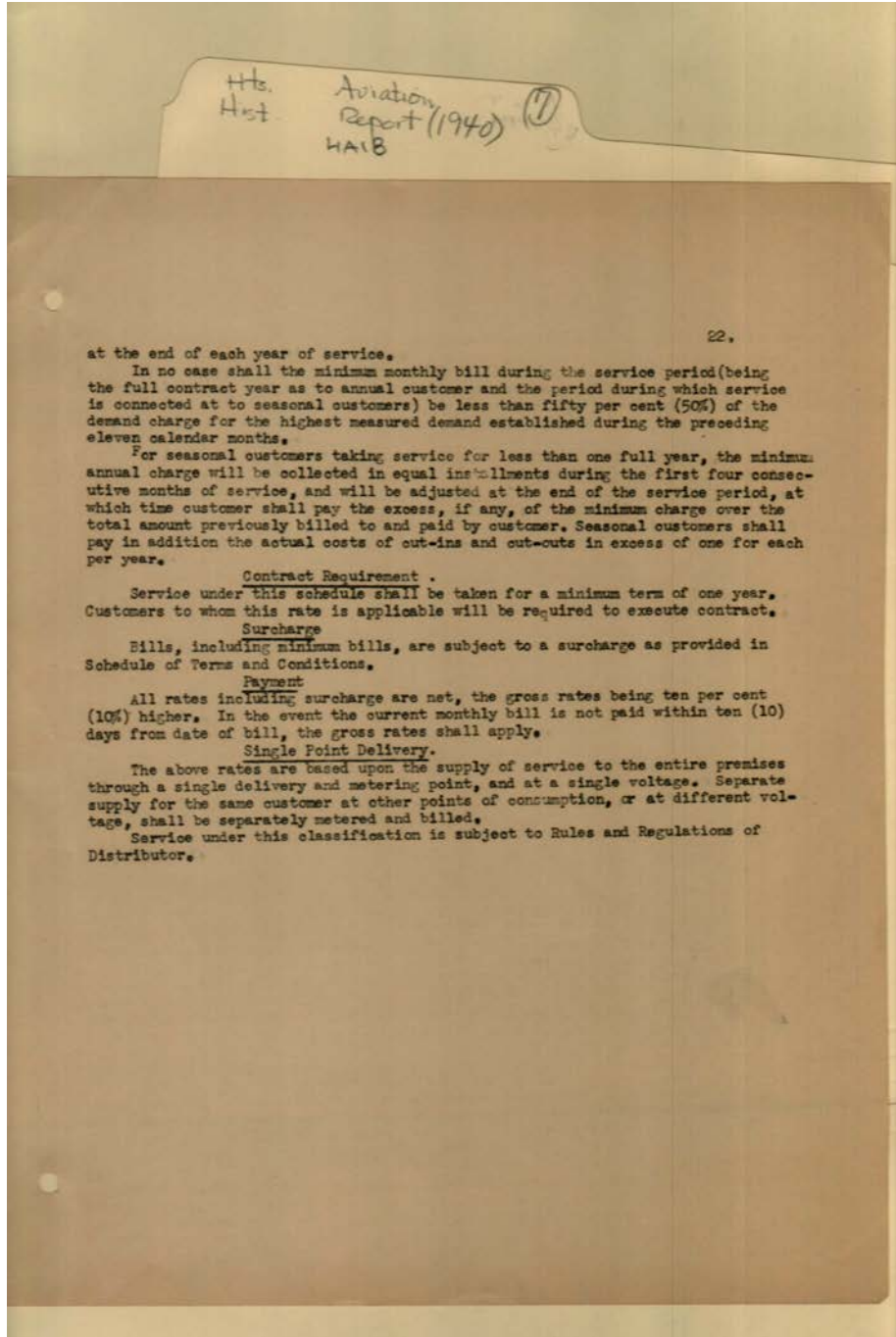
Types:

brief



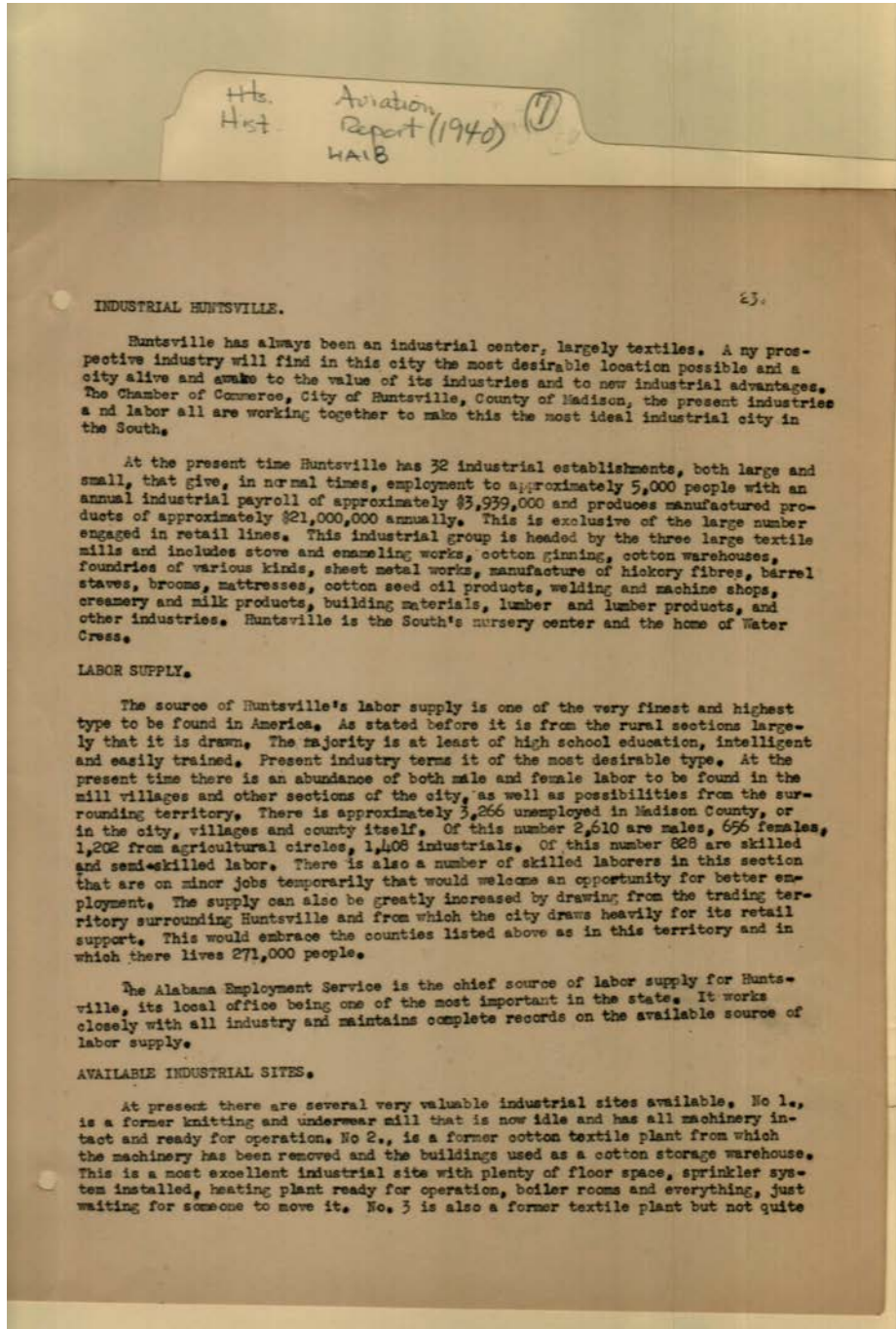
Names:
 Lighting and Power
 Costs

Types:
 brief



Names:
Costs

Types:
brief



Names:

Available Industrial
Sites

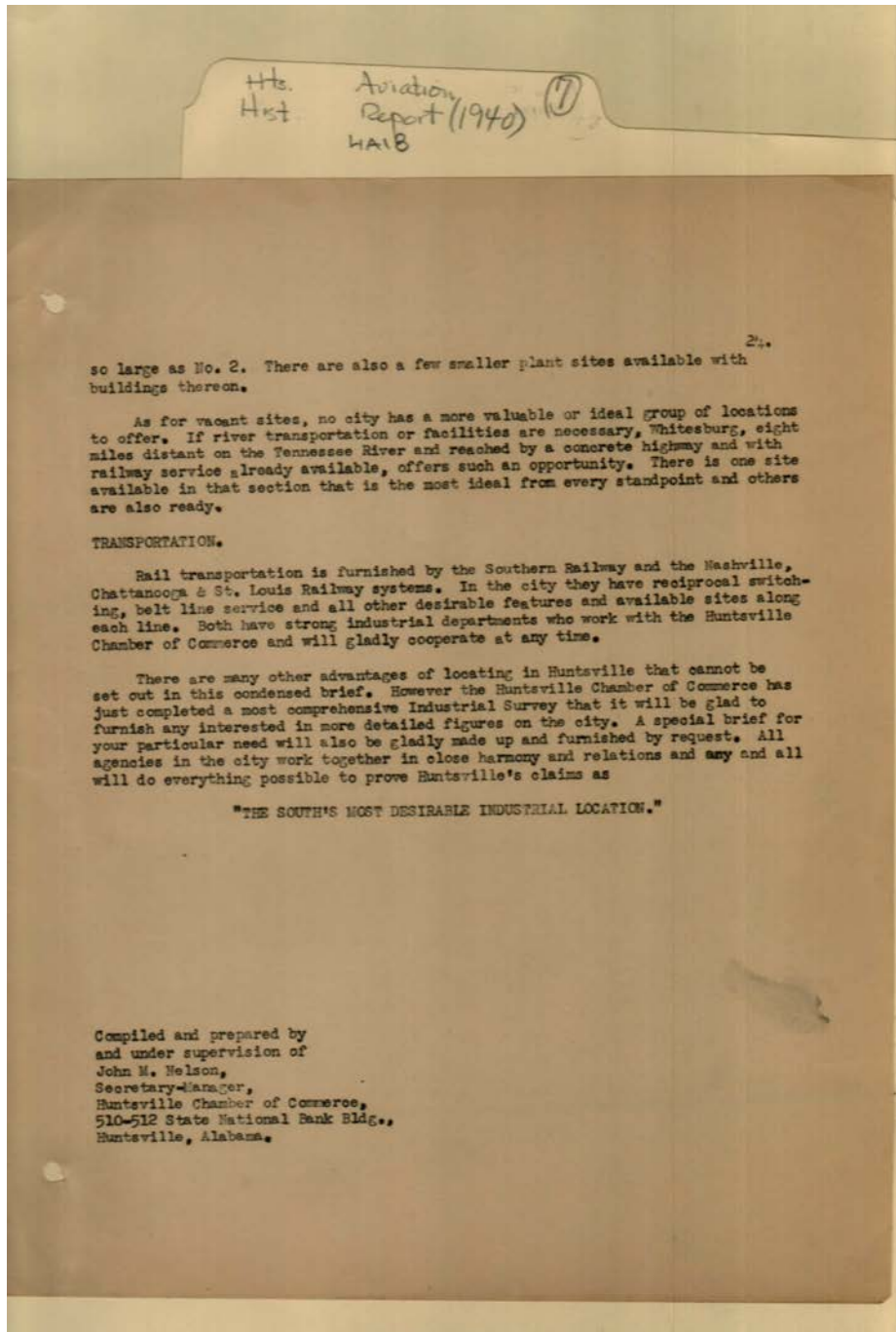
Industrial Huntsville
Labor Supply

Types:

brief

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7
Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

Image 71 r04a18-07-000-0137 [Contents](#) [Index](#) [About](#)



Names:

Huntsville Chamber
of Commerce

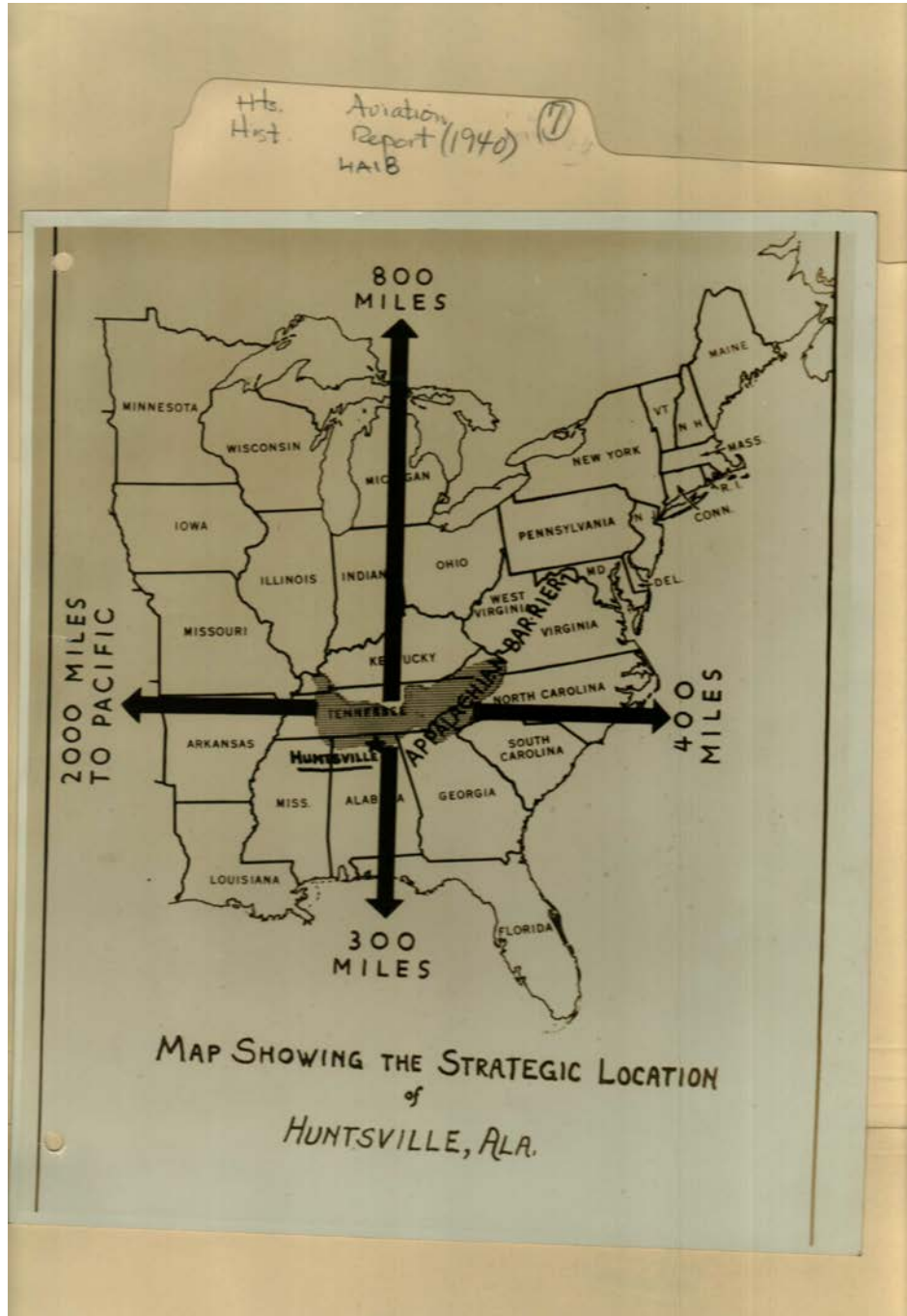
Nelson, John M.

Places:

Huntsville, AL

Types:

correspondence



Names:

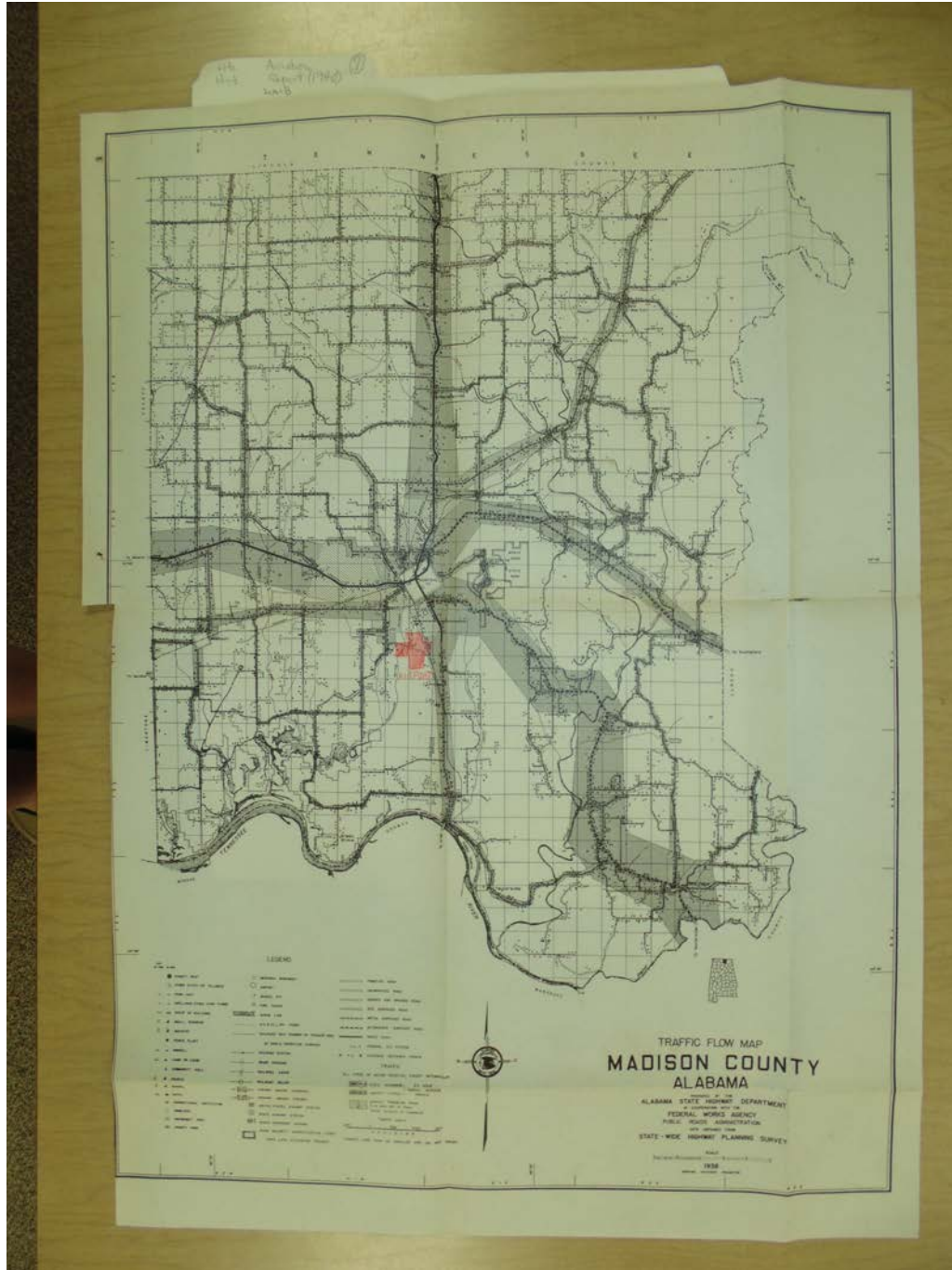
Huntsville Strategic
Location

Places:

Huntsville, AL

Types:

map



Names:

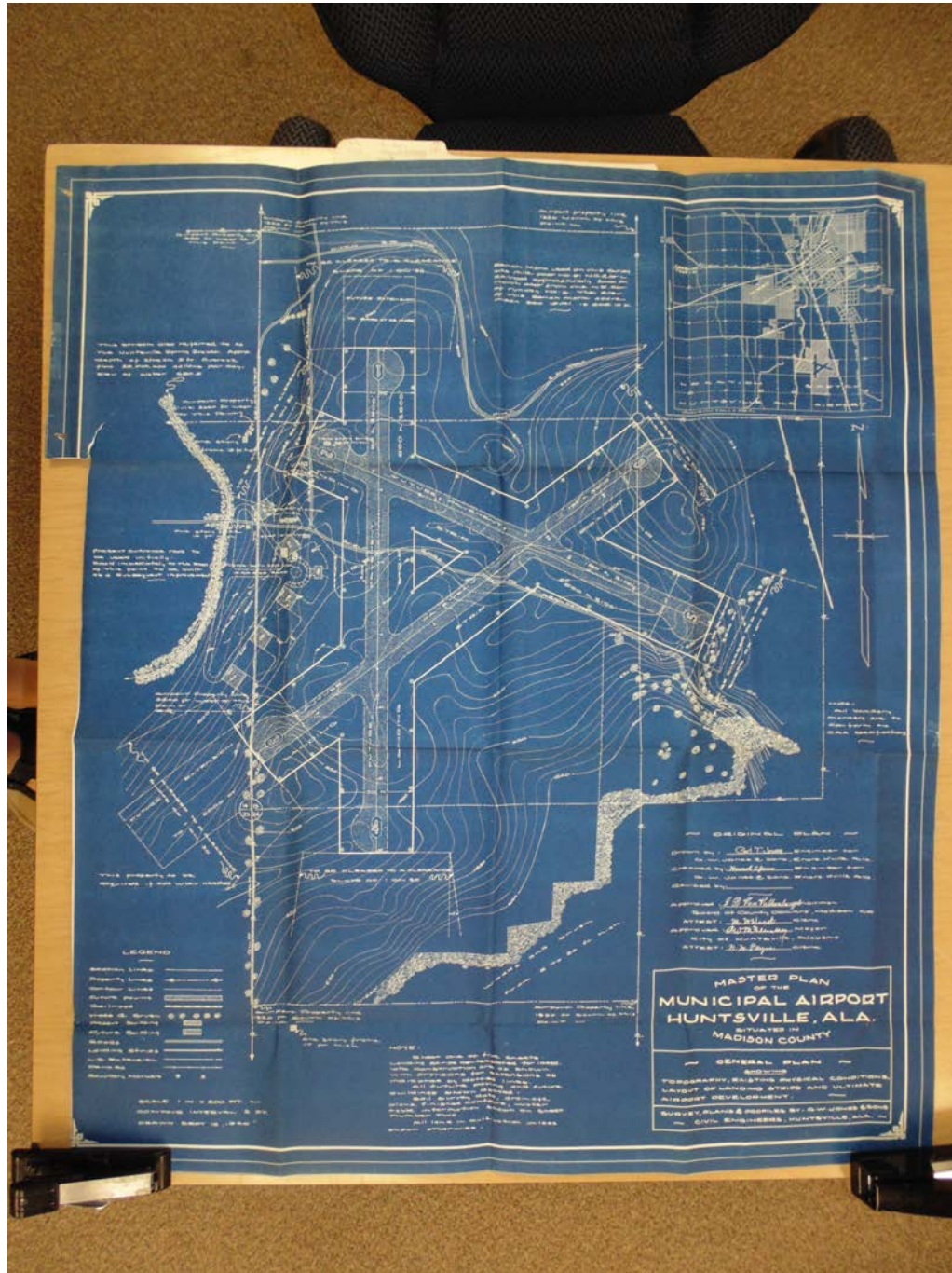
Madison County
Traffic Flow

Places:

Madison County, AL

Types:

map



Names:

Huntsville Municipal
Airport Master
Hurst, M. W.

Jones, Carl T.
Jones, Howard C.

McAllister, A. W.,
Mayor
Payne, N. M.

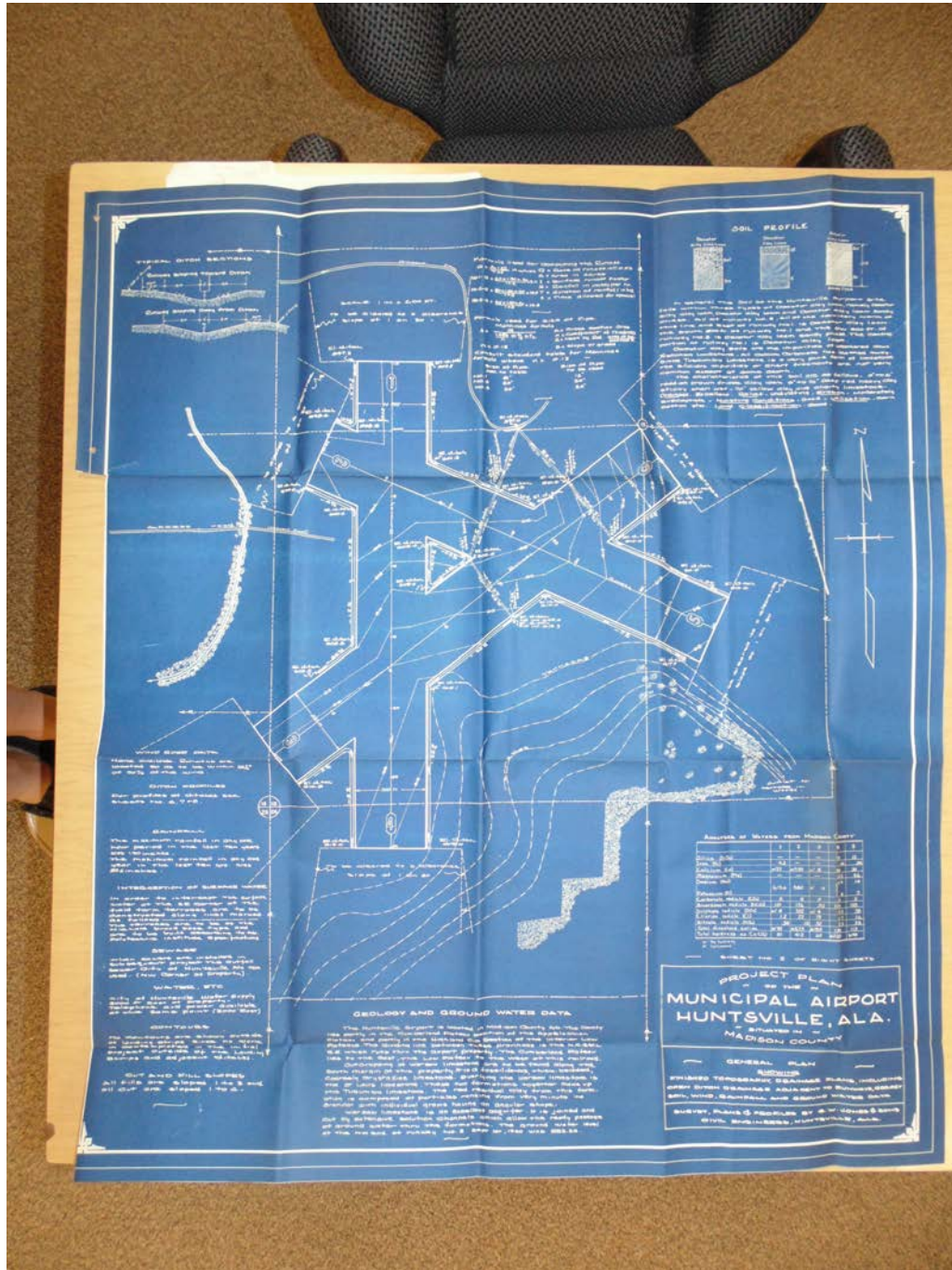
Van Valkenburgh, J.
B.

Places:

Huntsville, AL

Types:

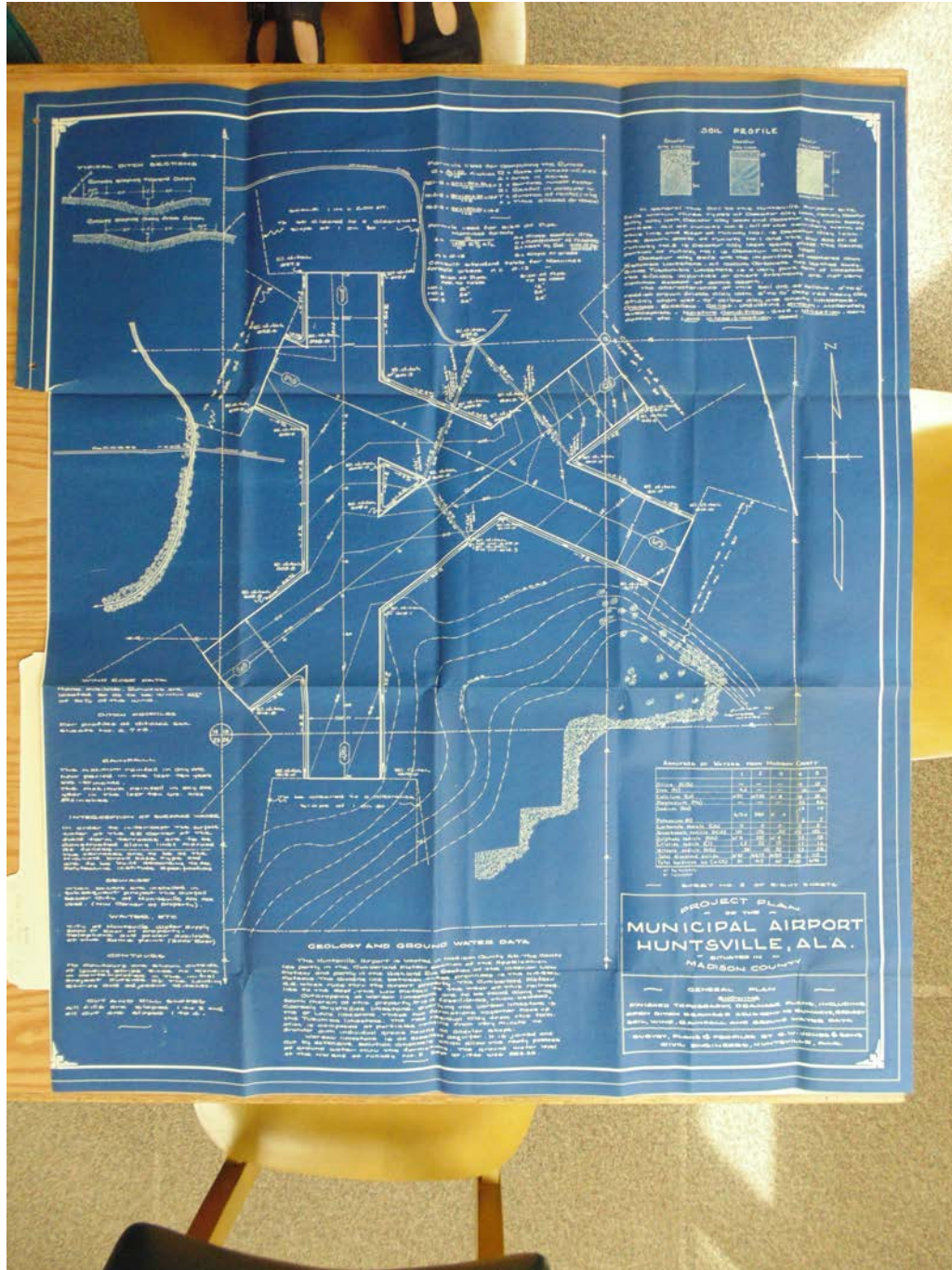
blue-line print



Names:
Huntsville Municipal
Airport

Places:
Huntsville, AL

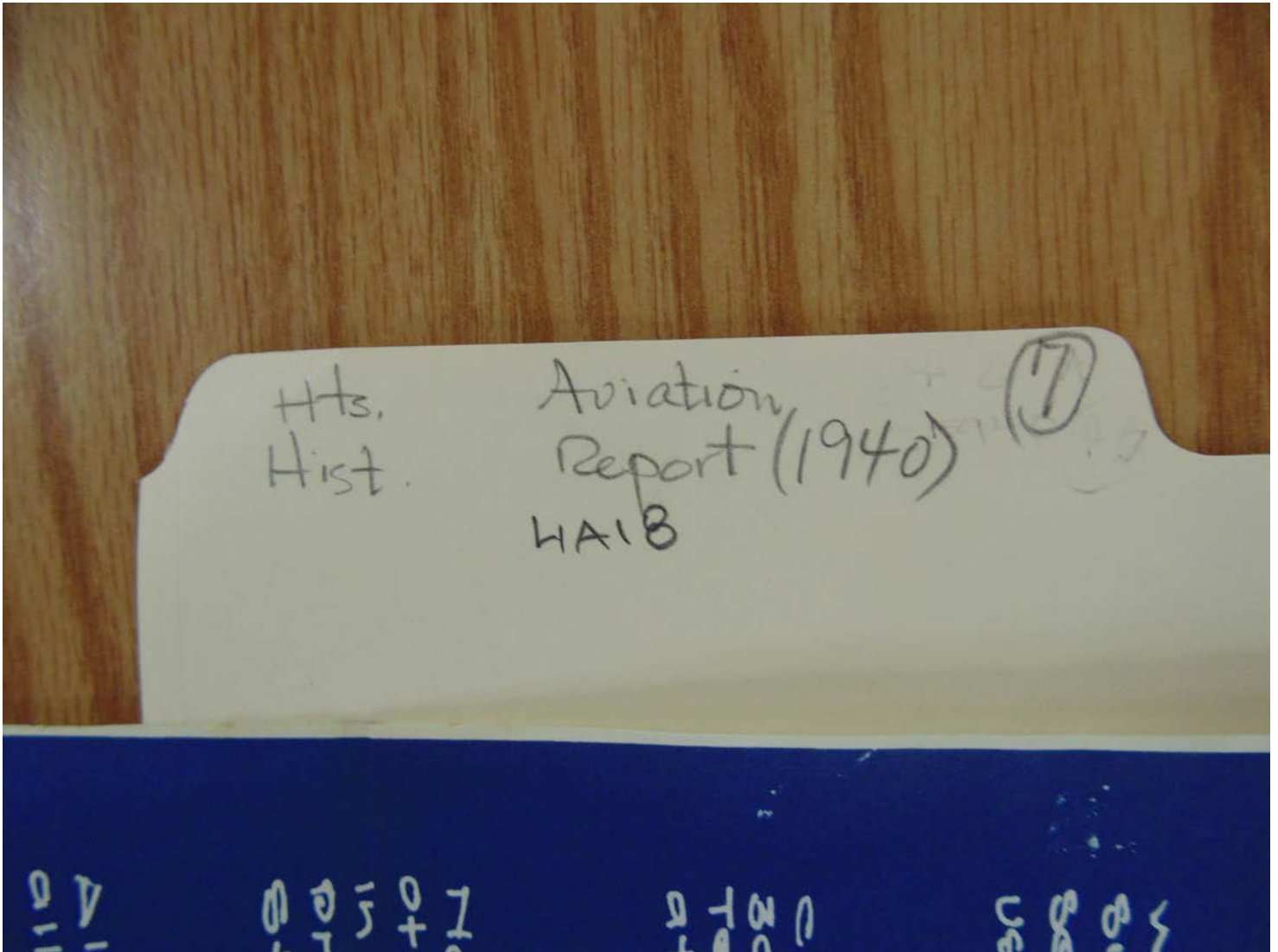
Types:
blueline print



Names:
 Huntsville Municipal
 Airport Project

Places:
 Huntsville, AL

Types:
 blueprint print



Names:

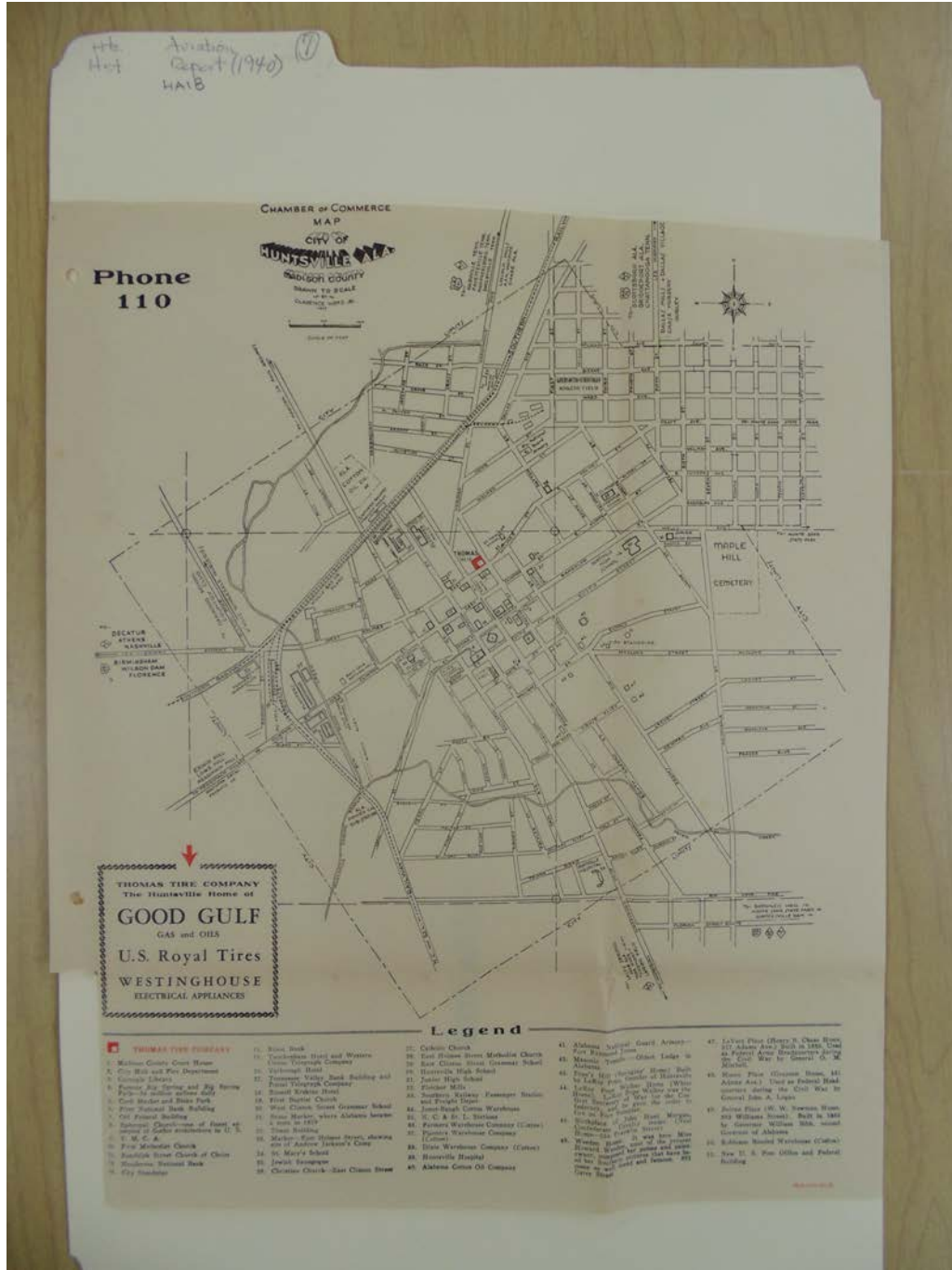
Title

Places:

Huntsville, AL

Types:

blueline print



Names:
 Huntsville, AL map

Thomas Tire
 Company

Places:
 Huntsville, AL

Types:
 map

**Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7
Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation**

[Contents](#) [Index](#) [About](#)

Table of Contents

[Image 1](#) (r04a18-07-000-0066)
[Image 2](#) (r04a18-07-000-0067)
[Image 3](#) (r04a18-07-000-0068)
[Image 4](#) (r04a18-07-000-0069)
[Image 5](#) (r04a18-07-000-0070)
[Image 6](#) (r04a18-07-000-0071)
[Image 7](#) (r04a18-07-000-0072)
[Image 8](#) (r04a18-07-000-0073)
[Image 9](#) (r04a18-07-000-0074)
[Image 10](#) (r04a18-07-000-0075)
[Image 11](#) (r04a18-07-000-0076)
[Image 12](#) (r04a18-07-000-0077)
[Image 13](#) (r04a18-07-000-0078)
[Image 14](#) (r04a18-07-000-0079)
[Image 15](#) (r04a18-07-000-0080)
[Image 16](#) (r04a18-07-000-0081)
[Image 17](#) (r04a18-07-000-0082)
[Image 18](#) (r04a18-07-000-0083)
[Image 19](#) (r04a18-07-000-0084)
[Image 20](#) (r04a18-07-000-0085)
[Image 21](#) (r04a18-07-000-0086)

[Image 22](#) (r04a18-07-000-0087)
[Image 23](#) (r04a18-07-000-0088)
[Image 24](#) (r04a18-07-000-0089)
[Image 25](#) (r04a18-07-000-0090)
[Image 26](#) (r04a18-07-000-0091)
[Image 27](#) (r04a18-07-000-0092)
[Image 28](#) (r04a18-07-000-0093)
[Image 29](#) (r04a18-07-000-0094)
[Image 30](#) (r04a18-07-000-0095)
[Image 31](#) (r04a18-07-000-0096)
[Image 32](#) (r04a18-07-000-0097)
[Image 33](#) (r04a18-07-000-0098)
[Image 34](#) (r04a18-07-000-0099)
[Image 35](#) (r04a18-07-000-0100)
[Image 36](#) (r04a18-07-000-0101)
[Image 37](#) (r04a18-07-000-0102)
[Image 38](#) (r04a18-07-000-0103)
[Image 39](#) (r04a18-07-000-0104)
[Image 40](#) (r04a18-07-000-0105)
[Image 41](#) (r04a18-07-000-0106)
[Image 42](#) (r04a18-07-000-0107)

[Image 43](#) (r04a18-07-000-0108)
[Image 44](#) (r04a18-07-000-0109)
[Image 45](#) (r04a18-07-000-0110)
[Image 46](#) (r04a18-07-000-0111)
[Image 47](#) (r04a18-07-000-0112)
[Image 48](#) (r04a18-07-000-0113)
[Image 49](#) (r04a18-07-000-0114)
[Image 50](#) (r04a18-07-000-0115)
[Image 51](#) (r04a18-07-000-0116)
[Image 52](#) (r04a18-07-000-0117)
[Image 53](#) (r04a18-07-000-0118)
[Image 54](#) (r04a18-07-000-0119)
[Image 55](#) (r04a18-07-000-0120)
[Image 56](#) (r04a18-07-000-0121)
[Image 57](#) (r04a18-07-000-0122)
[Image 58](#) (r04a18-07-000-0124)
[Image 59](#) (r04a18-07-000-0125)
[Image 60](#) (r04a18-07-000-0126)
[Image 61](#) (r04a18-07-000-0127)
[Image 62](#) (r04a18-07-000-0128)
[Image 63](#) (r04a18-07-000-0129)

[Image 64](#) (r04a18-07-000-0130)
[Image 65](#) (r04a18-07-000-0131)
[Image 66](#) (r04a18-07-000-0132)
[Image 67](#) (r04a18-07-000-0133)
[Image 68](#) (r04a18-07-000-0134)
[Image 69](#) (r04a18-07-000-0135)
[Image 70](#) (r04a18-07-000-0136)
[Image 71](#) (r04a18-07-000-0137)
[Image 72](#) (r04a18-07-000-0138)
[Image 73](#) (r04a18-07-000-4367)
[Image 74](#) (r04a18-07-000-4368)
[Image 75](#) (r04a18-07-000-4369)
[Image 76](#) (r04a18-07-000-4371)
[Image 77](#) (r04a18-07-000-4375)
[Image 78](#) (r04a18-07-000-4376)
[Table of Contents](#)
[Name & Place Index](#)
[About the Collection](#)

Frances Cabaniss Roberts Collection: Series 4, Subseries A, Box 18, Folder 7

Huntsville Aviation Reports and Resources, 1940 - 1941 - Transportation

[Contents](#) [Index](#) [About](#)

Name & Place Index

, Glenn [36](#)
About Huntsville [57](#)
Airport Soils [59](#)
Army Air Corps Officials [1](#)
Army Interests [23](#)
Army Post in Huntsville [56](#)
Atlanta, GA [3](#), [33](#), [34](#)
Available Industrial Sites [70](#)
Bailey, Josiah W. (Senator) [37](#)
Bankhead, John H. (Senator) [37](#)
Bankhead, John H. (Senator) [35](#)
Bell, Robert K. [1](#)
Bell, Verdon M. [35](#), [38](#), [39](#), [40](#)
Bower, A. H., [16](#), [42](#)
Bower, [17](#), [18](#)
Brown, J. S. [44](#)
Bryson, Joseph R. (congressman) [37](#), [38](#)
Bryson, [39](#), [40](#)
Bulwinkle, A. L. (congressman) [37](#)
Byrd, Harry F. (Senator) [37](#)
Chamber of Commerce [1](#)
Chattanooga is center TN Valley [11](#)
Chattanooga Location [10](#)
City of Huntsville [48](#)
Civil Aeronautics Board [3](#), [5](#)
Clapp, Gordon R. [22](#)
Climatic Data [63](#), [64](#)
Communities of Interest [15](#)
Conclusion [33](#)
Condensed Brief [61](#)
Costs [69](#)
Cotton Exchange [42](#)
Crews, T. W. [16](#)
Crews, [18](#)
Crumbliss, Raleigh [20](#)
Davis, Clifford (congressman) [37](#), [40](#), [41](#), [42](#)
Davis, [35](#)
Delta Routes [27](#)
Distance from Huntsville to Coast [49](#)
Durham, Carl T. (congressman) [37](#)
Eastern Air Lines, Inc. [3](#), [5](#)
Eastern Proposed a Service [24](#)
Eastern Routes [27](#)
Effect of Proposed Route [28](#)
Engleman, H. L. [23](#)
Evidence [7](#)
Expense Forecast [31](#)
Folger, John H. (congressman) [37](#)
Ford, John B. [3](#), [33](#)
Frank, John P. [35](#)
Gambrell, E. Smythe [3](#), [33](#), [34](#)
Glass, Carter (Senator) [37](#)
Glenn, L. M. [38](#)
Glenn, [35](#)
Government Costs [31](#)
Gray, J. W. [44](#)
Harlan, W. Glen [3](#), [33](#)
Hill, Lister [35](#), [37](#)
Hirshelmer, [21](#)
Huntsville Chamber of Commerce [71](#)
Huntsville Government [65](#)
Huntsville Information [47](#)
Huntsville Location [10](#), [54](#)
Huntsville Municipal Airport Master [74](#)
Huntsville Municipal Airport Project [76](#)
Huntsville Municipal Airport [58](#), [75](#)
Huntsville Population by Classes [62](#)
Huntsville Site for Aviation Industry [45](#)
Huntsville Strategic Location [72](#)
Huntsville, AL map [78](#)
Huntsville, AL [1](#), [2](#), [44](#), [45](#), [48](#), [53](#), [61](#), [62](#), [63](#), [67](#), [71](#),
[72](#), [74](#), [75](#), [76](#), [77](#), [78](#)
Hurst, M. W. [74](#)
Index [4](#)
Industrial Huntsville [70](#)
Introduction [46](#)
Johansen, [18](#)
Jones, Carl T. [74](#)
Jones, Howard C. [74](#)
Kefauver, Estes (congressman) [37](#)
Labor Supply [70](#)
Lighting and Power Costs [68](#)
Lucas, F. W. [41](#)
Lucas, Fred. W. [16](#)
Madison County Government [65](#)
Madison County Traffic Flow [73](#)
Madison County, AL [73](#)
Madison County [51](#)
Mann, C. O. [44](#)
Maps included [48](#)
Maybank, Bernet R. (Senator) [37](#)
McAllister, A. W., Mayor [2](#), [74](#)
McKellar, Kenneth [35](#), [37](#), [39](#), [41](#), [42](#), [43](#)
McLaurin, William [16](#)
Memphis Chamber of Commerce [35](#), [39](#), [40](#), [43](#)
Memphis to Chattanooga by air [19](#)
Memphis to Muscle Shoals [9](#)
Memphis, TN [35](#), [39](#), [40](#), [42](#)
Memphis-Carolina Service [26](#)
Memphis-Greenville Airline [37](#)
National Defense Area [52](#)
Navigation Aids Cost [32](#)
Need for Direct Transportation [21](#)
Nelson, John M. [71](#)

Parker, [31](#)
Payne, N. M. [74](#)
Pipes, Claude H. [35](#), [36](#)
Porter, General [26](#), [35](#), [38](#), [39](#), [40](#)
Porter, William M., General [22](#)
Proposed Service [6](#)
Public Hearing [42](#)
Public Need for Air Transportation [8](#), [25](#)
Public Utilities [66](#)
Quick, Joe [44](#)
Remaining Acreage for Building [60](#)
Reynolds, Robert R. (Senator) [37](#)
Rheinstrom, [21](#), [26](#), [27](#), [29](#)
Rountree, Asa, Jr. [58](#)
Satterfield, Dave E., Jr. (congressman) [37](#)
School Systems [66](#)
Smith, Ellison D. (Senator) [37](#)
Smith, George A. [3](#), [33](#)

Soderstrom, O. F. [36](#), [38](#)
Soderstrom, [17](#)
South Carolina needs [12](#)
Sparkman, John J. (congressman) [37](#)
Sparkman, John J. (congressman) [35](#)
Statistical Analysis [13](#)
Stewart, A. Tom [37](#)
Strategic Location [50](#), [53](#)
Tennessee Valley Area [55](#)
Thomas Tire Company [78](#)
Title [77](#)
Total Need [14](#)
Van Valkenburgh, J. B. [44](#), [74](#)
Varner, [43](#)
Warner, [35](#)
Washington, DC [38](#), [43](#)
Water and Gas Rates [67](#)

Frances Cabaniss Roberts Collection

Preferred Citation: Frances Cabaniss Roberts Collection, Archives and Special Collections, M. Louis Salmon Library, University of Alabama in Huntsville, Huntsville, AL.

Collection Scope and Content: The Collection of 114 Linear ft. includes a total of 156 Archival Boxes. The Frances Cabaniss Roberts collection covers the historical records of the Cabaniss Roberts family. This collection contains extensive correspondence records of the Cabaniss Roberts family circa 1830 to 1930.

Archives/Special Collections Access Restrictions: None

Conditions Governing Use: This material may be protected under U. S. Copyright Law (Title 17, U.S. Code) which governs the making of photocopies or reproductions of copyrighted materials. You may use the digitized material for private study, scholarship, or research. Though the University of Alabama in Huntsville Archives and Special Collections has physical ownership of the material in its collections, in some cases we may not own the copyright to the material. It is the patron's obligation to determine and satisfy copyright restrictions when publishing or otherwise distributing materials found in our collections.

Provenance: Gift of Johanna Shields on October 28, 2006.



THE UNIVERSITY OF
ALABAMA IN HUNTSVILLE

**The UAH Archives and Special Collections
M. Louis Salmon Library**